

TO: HONORABLE CHAIRMAN AND PLANNING COMMISSION

FROM: RON WHISENAND, COMMUNITY DEVELOPMENT DIRECTOR

**SUBJECT: CONDITIONAL USE PERMIT 07-019 / 1441 SPRING STREET
APN: 008-316-016 & -015 (APPLICANT: STEVE ELSAYED)**

DATE: APRIL 22, 2008

Needs: For the Planning Commission to consider a request to install a new automated car wash in conjunction with the rebuilding of an existing gas station.

Facts:

1. The property is located at 1441 Spring Street on the southwest corner of Spring Street and 15th Street. See vicinity map Attachment 1.
2. The site's General Plan designation is Community Commercial - Mixed Use Overlay (CC-MU), and it is zoned as Highway Commercial (C-2), which is consistent with the General Plan designation. The subject site is also located within the Downtown Historic District A.
3. The applicant proposes to demolish the existing gas station facility and construct a new facility.
4. In conjunction with the new facility, Mr. Elsayed proposes to install a 512 square foot automated carwash.
5. Table 21.16.200 of the Zoning Code requires the approval of a Conditional Use Permit (CUP) to establish a carwash in the (C-2) Zoning District.
6. The Development Review Committee (DRC) approved Site Plan 07-017 on October 15, 2007, which consisted of the approval of the site planning, parking, design and architecture of the buildings for the new gas station and convenience store. While the carwash was shown on the plans, it was not approved since it requires a conditional use permit.
7. The application before the Planning Commission is specifically to determine if the carwash is a compatible use for the site which is located adjacent to an established residential neighborhood to the west.
8. A noise study was prepared which identified noise impacts of the carwash operations and potential mitigations measures; see the Analysis and Conclusion section of the staff report for further discussion.

**Analysis
and**

Conclusion: The site has historically been used as a service station. The architecture of the proposed station will be an improvement to the Spring Street corridor. The central issue with this use permit is whether the location of a new car wash operation is appropriate given the close proximity of the established neighborhood to the west.

The carwash is proposed to be located at the North West corner of the property, approximately 40 feet to the west is the nearest sensitive receptor and approximately 20 feet from the residentially zoned property line. The cars are proposed to enter the carwash on the east side and exit into the alley which separates the carwash from the neighboring residential properties. According to the noise study, during operation (idling cars, scrubbers, blower, etc) the carwash could reach 87 decibels (dBA). With doors open, orientation of the carwash would direct sound towards the residential properties immediately west of the site across the alley.

The General Plan noise thresholds include different categories: normally acceptable; conditionally acceptable; normally unacceptable; and clearly unacceptable. Existing noise levels were measured from 4:00 p.m. to 5:00 p.m. on the afternoon of 1/25/08 at the subject site property line and alley (see the Noise Study in attachment 2). Ambient noise (traffic on Spring Street) on average was normally acceptable. Ambient noise proposed during operation of the propane tanks and the air compressor averaged at the boarder line of normally acceptable and conditionally acceptable levels.

The proposed project without the car wash, which includes relocation of the air compressor and propane tanks, will result in average ambient noise levels that are normally acceptable. With the addition of the car wash the ambient noise level during operation is considered to be unacceptable at ten feet away according to the General Plan's Noise Compatibility Index. However, mitigation measures such as the installation of Ryko bifold doors and limits on the hours of operation have been identified (7 a.m. to 7 p.m.) by the noise study as a measure that could reduce the noise to a conditionally acceptable level. For the bifold doors to be effective they would need to be closed during all car wash operations.

The proposed carwash potentially meets the purpose of the Historic District A's design guidelines by providing a limited service in the downtown area, therefore enhancing and expanding the convenience of services in the downtown area. However, the proposed site design, which sets the building back from the street, and idling and washing of cars, would be inconsistent with a pedestrian oriented atmosphere for the downtown.

While it does appear that carwash noise can be attenuated to lower dB levels, it still will introduce a new auditory source into a residential neighborhood. While the use may be appropriate in a commercial zone, the location next to residential properties raises the questions whether the carwash is appropriate at this location. Section 21.23.100 of the Municipal Code states that the purpose of a conditional use permit is to determine whether the use or business is "compatible with their environs depending upon the circumstances of the individual case."

The Planning Commission should consider whether or not the carwash is compatible with the adjacent residential land uses, and if this type of use is consistent with the Historic District A Guidelines. Approving the carwash, would require the Planning Commission is making the findings that the use will not be detrimental to the health, safety, morals, comfort, convenience, and general welfare of the persons residing in the neighborhood and a determination that a car wash can be considered

conditionally acceptable in terms of the General Plan's Noise Element and Land Use Element.

Policy

Reference: The City of Paso Robles General Plan and EIR, Paso Robles Zoning Ordinance, Economic Strategy, CEQA, Noise Study prepared by the Morro Group, Inc.

Fiscal

Impact: None.

Options: After opening the public hearing and taking public testimony, the Planning Commission is requested to take one of the actions listed below:

1. Adopt the attached Resolution denying Conditional Use Permit 07-019.
2. A. Adopt the attached Resolution approving the Mitigated Negative Declaration;
B. Adopt the attached Resolution approving Conditional Use Permit 07-017 provided required compatibility findings can be made; or
3. Amend, modify, or reject the above-listed action.

Attachments:

1. Vicinity Map
2. Resolution to Approve Mitigated Negative Declaration
3. Resolution to Deny the Conditional Use Permit 07-017
4. Resolution to Approve the Conditional Use Permit 07-017
5. Newspaper and Mail Notice Affidavits

RESOLUTION NO:

**A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF PASO ROBLES
ADOPTING MITIGATED NEGATIVE DECLARATION FOR
CONDITIONAL USE PERMIT 07-019
FOR PROPERTY LOCATED AT 1441 SPRING STREET
APNs : 008-316-016 AND -015
APPLICANT – STEVE ELSAYED**

WHEREAS, Conditional Use Permit (CUP) 07-019 has been filed by Cebulla Associates on behalf of Steve Elsayed; and

WHEREAS, CUP 07-019 is an application to establish an automated carwash service at 1441 Spring Street; and

WHEREAS, the project is consistent with the General Plan land use designation Community Commercial / Mixed Use Overlay (CC/M-U) and the Zoning district which is Highway Commercial-Planned Development / Mixed Use Overlay (C2-PD/M-U); and

WHEREAS, an Initial Study was prepared for this project (attached as Exhibit A) which concludes and proposes that a Mitigated Negative Declaration be approved; and

WHEREAS, pursuant to the Statutes and Guidelines of the California Environmental Quality Act (CEQA), and the City's Procedures for Implementing CEQA, an Initial Study and a Draft Mitigated Negative Declaration was prepared and circulated for public review and comment; and

WHEREAS, no public comments or responses were received in regard to the Draft Mitigated Negative Declaration and Initial Study; and

WHEREAS, Public Notice of the proposed Draft Mitigated Negative Declaration was posted as required by Section 21092 of the Public Resources Code; and

WHEREAS, a public hearing was conducted by the Planning Commission on April 22, 2008 to consider the Initial Study, the proposed Mitigated Negative Declaration prepared for the proposed project, and to accept public testimony on the application and environmental determination; and

WHEREAS, based on the information and analysis contained in the Initial Study (Exhibit A) prepared for this project and testimony received as a result of the public notice, the Planning Commission finds that there is no substantial evidence that there would be a significant impact on the environment as a result of the development and operation of the proposed project.

NOW, THEREFORE, BE IT RESOLVED, by the Planning Commission of the City of El Paso de Robles, based on its independent judgment, that it does hereby adopt a Mitigated Negative Declaration for CUP 07-019 in accordance with the Statutes and Guidelines of the California Environmental Quality Act (CEQA) and the City's Procedures for Implementing CEQA.

Exhibit A: Initial Study

PASSED AND ADOPTED THIS 22nd day of April 2008, by the following roll call vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

CHAIRMAN ED STEINBECK

ATTEST:

RON WHISENAND, PLANNING COMMISSION SECRETARY

CITY OF PASO ROBLES – PLANNING DIVISION INITIAL STUDY

1. GENERAL PROJECT INFORMATION

PROJECT TITLE: Car Wash – Conditional Use Permit 07-019, Site Plan 07-017

LEAD AGENCY: City of Paso Robles - 1000 Spring Street, Paso Robles, CA 93446

Contact: Mathew Fawcett, Assistant Planner
Telephone: (805) 237-3970

PROJECT LOCATION: 1441 Spring Street, Paso Robles, CA 93446 (APN 008-316-016 & -015)

PROJECT PROPONENT: Applicant: Steve Elsayed
1441 Spring Street, Paso Robles, CA 93446
Representative: Cebulla Associates

**LEAD AGENCY CONTACT/
INITIAL STUDY PREPARED BY:** Mathew Fawcett, Assistant Planner

Telephone: (805) 237-3970
E-Mail: mfawcett@prcity.com

GENERAL PLAN DESIGNATION: Community Commercial / Mixed Use Overlay (CC/MU)

ZONING: Highway Commercial / Mixed Use Overlay (C2/MU)

2. PROJECT DESCRIPTION

The proposed project is a request to install a new automated car wash in conjunction with the rebuilding of an existing gas station. This request includes the reconstruction of the convenience store and relocation to the southwest corner of the site and changing the location of the gas pumps, propane dispenser, and air-compressor.

3. **OTHER AGENCIES WHOSE APPROVAL MAY BE REQUIRED (For example, issuance of permits, financing approval, or participation agreement):**

San Luis Obispo County Air Pollution Control District.

4. **EARLIER ENVIRONMENTAL ANALYSIS AND RELATED ENVIRONMENTAL DOCUMENTATION:**

This Initial Study incorporates by reference the City of El Paso de Robles General Plan Environmental Impact Report (EIR) (SCH#2003011123).

5. **CONTEXT OF ENVIRONMENTAL ANALYSIS FOR THE PROJECT:**

This Initial Study relies on expert opinion supported by the facts, technical studies, and technical appendices of the City of El Paso de Robles General Plan EIR. These documents are incorporated herein by reference. They provide substantial evidence to document the basis upon which the City has arrived at its environmental determination regarding various resources.

6. PURPOSES OF AN INITIAL STUDY

The purposes of an Initial Study for a Development Project Application are:

- A. To provide the City with sufficient information and analysis to use as the basis for deciding whether to prepare an Environmental Impact Report, a Mitigated Negative Declaration, or a Negative Declaration for a site specific development project proposal;
- B. To enable the Applicant of a site specific development project proposal or the City as the lead agency to modify a project, mitigating adverse impacts before an Environmental Impact Report is required to be prepared, thereby enabling the proposed Project to qualify for issuance of a Negative Declaration or a Mitigated Negative Declaration;
- C. To facilitate environmental assessment early in the design of a project;
- D. To eliminate unnecessary EIRs;
- E. To explain the reasons for determining that potentially significant effects would not be significant;
- F. To determine if a previously prepared EIR could be used for the project;
- G. To assist in the preparation of an Environmental Impact Report if one is required; and
- H. To provide documentation of the factual basis for the finding of no significant effect as set forth in a Negative Declaration or a Mitigated Negative Declaration prepared for the a project.

7. EXPLANATION OF ANSWERS FOUND ON THE ENVIRONMENTAL CHECKLIST FORM

A. Scope of Environmental Review

This Initial Study evaluates potential impacts identified in the following checklist.

B. Evaluation of Environmental Impacts

- 1. A brief explanation is required for all answers to the questions presented on the following Environmental Checklist Form, except where the answer is that the proposed project will have “No Impact.” The “No Impact” answers are to be adequately supported by the information sources cited in the parentheses following each question or as otherwise explained in the introductory remarks. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to the project. A “No Impact” answer should be explained where it is based on project-specific factors and/or general standards. The basis for the “No Impact” answers on the following Environmental Checklist Form is explained in further detail in this Initial Study in Section 9 (Earlier Environmental Analysis and Related Environmental Documentation) and Section 10 (Context of Environmental Analysis for the Project).
- 2. All answers on the following Environmental Checklist Form must take into account the whole action involved with the project, including implementation. Answers should address off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
- 3. “Potentially Significant Impact” is appropriate, if an effect is significant or potentially significant, or if the lead agency lacks information to make a finding of insignificance. If there are one or more

“Potentially Significant Impact” entries when the determination is made, preparation of an Environmental Impact Report is warranted.

4. Potentially Significant Impact Unless Mitigated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level. Mitigation Measures from Section 9 (Earlier Environmental Analysis and Related Environmental Documentation) may be cross-referenced).
5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). See Section 4 (Earlier Environmental Analysis and Related Environmental Documentation) and Section 11 (Earlier Analysis and Background Materials) of this Initial Study.
6. References to the information sources for potential impacts (e.g., general plans, zoning ordinances) have been incorporated into the Environmental Checklist Form. See Section 11 (Earlier Analysis and Related Environmental Documentation). Other sources used or individuals contacted are cited where appropriate.
7. The following Environmental Checklist Form generally is the same as the one contained in Title 14, California Code of Regulations; with some modifications to reflect the City’s needs and requirements.
8. Standard Conditions of Approval: The City imposes standard conditions of approval on Projects. These conditions are considered to be components of and/or modifications to the Project and some reduce or minimize environmental impacts to a level of insignificance. Because they are considered part of the Project, they have not been identified as mitigation measures. For the readers’ information, the standard conditions identified in this Initial Study are available for review at the Community Development Department.
9. Certification Statement: The statements made in this Initial Study and those made in the documents referenced herein present the data and information that are required to satisfy the provisions of the California Environmental Quality Act (CEQA) – Statutes and Guidelines, as well as the City’s Procedures for Implementing CEQA. Further, the facts, statements, information, and analysis presented are true and correct in accordance with standard business practices of qualified professionals with expertise in the development review process, including building, planning, and engineering.

8. ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The proposed project may potentially affect the environmental factors checked below, and may involve at least one impact that is a “Potentially Significant Impact” or is “Potentially Significant Unless Mitigated,” if so indicated on the following Environmental Checklist Form (Pages 8 to.15)

- | | | |
|---|---|--|
| <input checked="" type="checkbox"/> Land Use & Planning | <input type="checkbox"/> Transportation/Circulation | <input type="checkbox"/> Public Services |
| <input type="checkbox"/> Population & Housing | <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Utilities & Service Systems |
| <input type="checkbox"/> Geological Problems | <input type="checkbox"/> Energy & Mineral Resources | <input type="checkbox"/> Aesthetics |
| <input checked="" type="checkbox"/> Water | <input type="checkbox"/> Hazards | <input type="checkbox"/> Cultural Resources |
| <input type="checkbox"/> Air Quality | <input checked="" type="checkbox"/> Noise | <input type="checkbox"/> Recreation |
| | <input type="checkbox"/> Mandatory Findings of Significance | |

9. ENVIRONMENTAL DETERMINATION: On the basis of this initial evaluation: I find that:

The proposed project could not have a significant effect on the environment; and, therefore, a **NEGATIVE DECLARATION** will be prepared.

Although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on an attached sheet have been added to the project. Therefore, a **MITIGATED NEGATIVE DECLARATION** will be prepared.

The proposed project may have a significant effect on the environment; and, therefore an **ENVIRONMENTAL IMPACT REPORT** is required.

The proposed project may have a significant effect(s) on the environment, but one or more effects (1) have been adequately analyzed in an earlier document pursuant to applicable legal standards, and (2) have been addressed by mitigation measures based on the earlier analysis as described on attached sheets, if the effect is a “potentially significant impact” or is “potentially significant unless mitigated.”

Therefore, an **ENVIRONMENTAL IMPACT REPORT** is required, but it will analyze only the effect or effects that remain to be addressed.

Signature:

Date:

April 2, 2008

Mathew Fawcett, Assistant Planner

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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I. LAND USE AND PLANNING. Would the Proposal:

- a) Conflict with general plan designation or zoning?
(Sources: 1, 11)

Discussion: The proposed project would conflict with the Noise Element's thresholds established in the 2003 General Plan/General Plan EIR unless mitigated. With mitigation measures incorporated the project would be in compliance with the General Plan and therefore the potential impacts would then be less than significant. See Sec. Xa. for full discussion.

- b) Conflict with applicable environmental plans or policies adopted by agencies with jurisdiction over the project?
(Sources: 1 & 3)

Discussion: The proposed project complies with the EIR recently certified for the City General Plan Update, 2003 and other adopted environmental policies that apply to this project therefore no significant impacts are anticipated with this project.

- c) Be incompatible with existing land uses in the vicinity?
(Sources: 1, 3, & 11)

Discussion: The surrounding uses include a mix of commercial to the north, south, and east, and residential to the west. The proposed project would likely be incompatible with the adjacent residential land use (Duplex, Triplex / R-2) to the west due to the noise the carwash would produce. However, mitigation measures have been added to bring the car wash into compliance with the Noise Element, including installation of bifold doors to baffle noise resulting from this use. Therefore, noise impacts that would otherwise be incompatible with the adjacent residential land use would be mitigated to a less than significant use.

- d) Affect agricultural resources or operations (e.g., impacts to soils or farmlands, or impacts from incompatible uses)?

Discussion: The project site is an urban infill property with no agricultural uses, resources or operations on or near the property, therefore there are no anticipated impacts to agricultural resources or operations.

- e) Disrupt or divide the physical arrangement of an established community (including a low-income or minority community)?
(Sources: 1 & 3)

Discussion: The site is already developed with an existing gas station, and with mitigation measures incorporated the proposed project would not disrupt or divide the physical arrangement of an established community. Therefore the project would not result in impacts related to this issue.

II. POPULATION AND HOUSING. Would the proposal:

- a) Cumulatively exceed official regional or local population projections? (Sources: 1 & 3)

Discussion: The proposed project does not propose any new housing, therefore the project would not result in impacts to the established population thresholds.

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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- b) Induce substantial growth in an area either directly or indirectly (e.g., through projects in an undeveloped area or extension of major infrastructure)? (Sources: 1 & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: This is an existing infill property. The project will not extend major infrastructure that would induce substantial growth since there are existing services and infrastructure surrounding the property, therefore the project would not impact growth.

- c) Displace existing housing, especially affordable housing? (Sources: 1, 3, & 5)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: There is currently an existing gas station and convenience store on the project site, thus, the project will not displace existing housing.

III. GEOLOGIC PROBLEMS. Would the proposal result in or expose people to potential impacts involving:

- a) Fault rupture? (Sources: 1, 2, & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Discussion: The potential for and mitigation of impacts that may result from fault rupture in the project area are identified and addressed in the General Plan EIR, pg. 4.5-8. There are two known fault zones on either side of this valley. The Rinconada Fault system runs on the west side of the valley. The San Andreas Fault is on the east side of the valley and runs through the community of Parkfield east of Paso Robles. The City of Paso Robles recognizes these geologic influences in the application of the California Building Code to all new development within the City. Review of available information and examinations indicate that neither of these faults is active with respect to ground rupture in Paso Robles. Soils reports and structural engineering in accordance with local seismic influences would be applied in conjunction with any new development proposal. Based on standard conditions of approval, the potential for fault rupture and exposure of persons or property to seismic hazards is not considered significant. In addition, per requirements of the Alquist-Priolo Earthquake Fault Zones, only structures for human habitation need to be setback a minimum of 50 feet of a known active trace fault.

- b) Seismic ground shaking? (Sources: 1, 2, & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Discussion: The City is located within an active earthquake area that could experience seismic ground shaking from the Rinconada and San Andreas Faults. The proposed structure will be constructed to current CBC codes. The General Plan EIR identified impacts resulting from ground shaking as less than significant and provided mitigation measures that will be incorporated into the design of this project including adequate structural design and not constructing over active or potentially active faults.

- c) Seismic ground failure, including liquefaction? (Sources: 1, 2 & 3)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: Per the General Plan EIR, the project site is located in an area with soil conditions that have a potential for liquefaction or other type of ground failure due to seismic events due to soil conditions. The EIR identifies measures to reduce this potential impact, which will be incorporated into this project. This includes a requirement to conduct a site-specific analysis of liquefaction potential. Based on analysis results, the project design and construction will include specific design requirements to reduce the potential impacts on structures due to liquefaction to a less than significant

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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level.

d) Seiche, tsunami, or volcanic hazard? (Sources: 1, 2, & 3)

e) Landslides or Mudflows? (Sources: 1, 2, & 3)

Discussion: d. and e. The project site is not located near bodies of water or volcanic hazards, nor is the site located in an area subject to landslides or mudflows.

f) Erosion, changes in topography or unstable soil conditions from excavation, grading, or fill? (Sources: 1, 2, 3, & 4)

Discussion: Per the General Plan EIR the soil condition is not erosive or otherwise unstable. As such, no significant impacts are anticipated. The site is relatively flat and will need minimal grading

g) Subsidence of the land? (Sources: 1, 2, & 3)

Discussion: See Item c.

h) Expansive soils? (Sources: 4)

Discussion: Per the General Plan EIR, Paso Robles is an area that has moderately expansive soils. This issue will be addressed through implementation of appropriate soil preparation as determined necessary by recommendations of site specific soils report. Therefore, impacts related to expansive soils will be less than significant.

i) Unique geologic or physical features? (Sources:1 & 3)

Discussion: There are no unique geologic or physical features on or near the project site.

IV. WATER. Would the proposal result in:

a) Changes in absorption rates, drainage patterns, or the rate and amount of surface runoff? (Sources:1, 3, & 7)

Discussion: The project site is currently developed with an existing gas station. With the rebuild of the gas station and the addition of the carwash does not significantly change the absorption rate, drainage pattern, or rate and amount of surface runoff. See attachment 3 &4 Existing Site Plan and Preliminary Grading Plan.

b) Exposure of people or property to water related hazards such as flooding? (Sources: 1, 3, & 7)

Discussion: There is no potential to expose people or property to water related hazards due to this project since it is not
Initial Study-Page 7

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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in a flood zone.

- c) Discharge into surface waters or other alteration of surface water quality (e.g., temperature, dissolved oxygen or turbidity)? (Sources: 1, 3, 7, & Attachment 7, 8)

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Discussion: The carwash is proposing to use the RYKO Environmentalist II-A reclaim system that reuses and treats water used from previous washes as well as utilize a reverse osmosis spot-free rinse instead of a chemical treated spot free rinse. In addition to the RYKO Environmentalist II-A the water discharge from this project will be required to meet all Municipal codes, CBC codes, and the National Pollution Discharge Elimination System (NPDES) requirements relating to water discharge. With compliance with these required regulations and as a condition of approval, this project will not result in impacts to water quality.

- d) Changes in the amount of surface water in any water body? (Sources: 1, 3, & 7)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: There is no water body on or near the project site, therefore the project could not impact surface water.

- e) Changes in currents, or the course or direction of water movement? (Sources: 1, 3, & 7)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: This project could not result in changes in currents or water movement since there is no water course in the vicinity that could be affected by this project.

- f) Change in the quantity of ground waters, either through direct additions or withdrawals, or through interception of an aquifer by cuts or excavations or through substantial loss of groundwater recharge capability? (Sources: 1,3, & 7)

<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
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Discussion: The proposed project does not directly withdraw water resources. The project is consistent with the build-out scenario in the General Plan and planned water use and reserve capacity.

- g) Altered direction or rate of flow of groundwater? (Sources: 1, 3, & 7)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: This project could not result in alterations to the direction or rate of groundwater flow since this project does not directly extract groundwater or otherwise significantly affect these resources.

- h) Impacts to groundwater quality? (Sources: 1, 3, & 7)

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion: The project will not affect groundwater quality since this project does not directly extract groundwater or otherwise affect these resources, and the proposed uses do not utilize materials or methods that would result in reduced groundwater quality. This project will not change existing water quality from discharging in surface waters with implementation of standard storm water discharge infrastructure that is in compliance with the National Pollution Discharge Elimination System (NPDES) requirements.

- i) Substantial reduction in the amount of groundwater otherwise

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
available for public water supplies? (Sources: 1, 3, & 7)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion: Refer to response f.

V. AIR QUALITY. Would the proposal:

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| a) Violate any air quality standard or contribute to an existing or projected air quality violation? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: The proposed use is less than the APCD level of significant threshold with the addition of the following mitigation measure the potential impacts to air quality standards or contribution to an existing air quality violation are consider less than significant.

AQ-2: Prior to issuance of a construction permit, the subject project shall be in compliance with the SLO Co. Air Pollution Control District and the San Luis Obispo County Clean Air Plan standard and discretionary conditions of approval.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| b) Expose sensitive receptors to pollutants? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: There are no sensitive receptors such as schools, hospitals, etc. within the near vicinity that could be impacted by this project.

- | | | | | |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|
| c) Alter air movement, moisture, or temperature?
(Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: This project does not have the potential to significantly alter air movement, moisture, or temperature since the project is a small scale redevelopment project.

- | | | | | |
|--------------------------------|--------------------------|--------------------------|-------------------------------------|--------------------------|
| d) Create objectionable odors? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|--------------------------------|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion: Given the nature of the proposed uses, this project does generally have the potential to create objectionable odors from idling cars and gasoline, however the following mitigation measure reduces any potential impacts to less than significant:

AQ-2: Prior to issuance of a construction permit, the subject project shall be in compliance with the SLO Co. Air Pollution Control District and the San Luis Obispo County Clean Air Plan standard and discretionary conditions of approval.

VI. TRANSPORTATION/CIRCULATION. Would the proposal result in:

- | | | | | |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|
| a) Increased vehicle trips or traffic congestion?
(Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
|---|--------------------------|--------------------------|-------------------------------------|--------------------------|

Discussion. The uses proposed are consistent with the General Plan's land use designation and the Zoning Ordinance's zoning designation. The additional trips are generated by the use do not impact the Level of Service (LOS) along Spring Street. The trip generation for the convenience store, gas station, and carwash has been calculated using rates contained

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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in the Trip Generation Manual, 6th Edition, Institute of Transportation Engineers. The average weekday trips generated for this project will result in approximately 1,223 average daily trips (ADT), and 85 AM peak hour and 106 PM peak hour trips. The existing level of service (LOS - volume to capacity) of the surrounding street network and intersections (and with planned mitigation improvements per General Plan EIR, 2003) are LOS B. The addition of the trips that are anticipated to be generated by this project will be less than significant on the surrounding network since the additional trip generation volume resulting from this project will not exceed the design capacity for local, collector or arterial streets identified in the General Plan Circulation Element.

- b) Hazards to safety from design features (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? (Sources: 1, 3, & 7)

Discussion: The proposed project does not include road improvements that may result in safety hazards or in incompatible uses.

- c) Inadequate emergency access or inadequate access to nearby uses? (Sources: 1, 3, & 7)

Discussion: The project is adequately served by public streets for emergency services.

- d) Insufficient parking capacity on-site or off-site? (Sources: 1, 3, & 7)

Discussion: Per the Zoning Ordinance requirements, the site plan parking design and number of parking spaces complies with the Ordinance requirements for the proposed uses. The project requires 15 total parking spaces. The site plan shows 7 spaces. The 8 missing will be accounted for through the downtown parking in-lieu fee program which addresses any potential parking impacts.

- e) Hazards or barriers for pedestrians or bicyclists? (Source: 7)

Discussion: The project does not have hazards or barriers for pedestrians or bicyclists.

- f) Conflicts with adopted policies supporting alternative transportation (e.g., bus turnouts, bicycle racks)? (Sources: 1 & 7)

Discussion: The project would not conflict with or otherwise affect adopted policies supporting alternative transportation.

- g) Rail, waterborne or air traffic impacts?

Discussion: The project could not affect rail, waterborne or air traffic.

BIOLOGICAL RESOURCES. Would the proposal result in impacts to:

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Endangered, threatened or rare species or their habitats (including but not limited to: plants, fish, insects, animals, and birds)?

Discussion: There are no endangered, threatened or rare species or their habitats located on the project site. The property is currently developed with an existing gas station. Thus, there could not be potential impacts to endangered, threatened or rare species or their habitats.

b) Locally designated species (e.g., heritage trees)?

Discussion: There are no locally designated species, including oak trees on the project site.

c) Locally designated natural communities (e.g., oak forest, coastal habitat, etc.)?

Discussion: See item b. above.

d) Wetland habitat (e.g., marsh, riparian and vernal pool)?

Discussion: There are no wetland habitats on or near the project site.

e) Wildlife dispersal or migration corridors?

Discussion: The site is not part of a wildlife dispersal or migration corridor.

VIII. ENERGY AND MINERAL RESOURCES. Would the proposal:

a) Conflict with adopted energy conservation plans? (Sources: 1 & 7)

Discussion: The structures will be designed and constructed according to applicable CBC codes and Title 24 energy conservation requirements, thus it will not conflict with adopted energy conservation plans.

b) Use non-renewable resources in a wasteful and inefficient manner? (Sources: 1 & 7)

Discussion: The project will not use non-renewable resource in a wasteful and inefficient manner.

c) Result in the loss of availability of a known mineral resource that would be of future value to the region and the residents of the State? (Sources: 1 & 7)

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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Discussion: The project is not located in an area of a known mineral resources that would be of future value to the region and the residents of the State.

IX. HAZARDS. Would the proposal involve:

- a) A risk of accidental explosion or release of hazardous substances (including, but not limited to: oil, pesticides, chemicals or radiation)?

Discussion: The project is required to meet all applicable Municipal Code requirements, therefore mitigating the project's risk of accidental explosion or release of hazardous substances since the uses do not generally use these types of substance to less than significant.

- b) Possible interference with an emergency response plan or emergency evacuation plan? (Sources: 1 & 7)

Discussion: The project will not interfere with an emergency response plan or emergency evacuation plan since it is not a designated emergency response location to be used for staging or other uses in an emergency.

- c) The creation of any health hazard or potential hazards?

Discussion: The project is required to meet all applicable Municipal Code requirements, therefore the project and future uses will not likely result in creating any health or other hazards, thus reducing the anticipated impacts to less than significant.

- d) Increased fire hazard in areas with flammable brush, grass, or trees?

Discussion: The project site is not located in an area with the potential for increased fire hazards. The site will be required to be in compliance with City and County brush and grass clearance requirements.

X. NOISE. Would the proposal result in:

- a) Increases in existing noise levels? (Sources: 1, 3, 7, & 11)

Discussion: A noise study was prepared for this project to assess potential noise impacts that may result from this project. According to the noise study report prepared by the Morro Group, Inc., the operation of the automated car wash would exceed the 70 dB threshold of the Community Commercial Land Use Category, by 17 dB and it would exceed the 65 dB threshold for the Residential Multi-Family Land Use Category by 19 dB. The report recommends two mitigation measures to mitigate the noise of the carwash so that it will be in compliance with City standards. See attached mitigation measures:

N-1: Prior to issuance of construction permits, the applicant shall submit revised plans showing the use of Ryko bifold doors at the entrance and exit of the car wash facility. The applicant shall submit documentation in the form of a noise diagram from the manufacturer verifying the statements made by Mike McGinness, which support the operational mitigation measure.

N-2: The applicant shall limit hours of car wash operation from 7 a.m. to 7 p.m. to avoid Community Noise Equivalency weighting factors.

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| b) Exposure of people to severe noise levels? (Source: 1, 3, 7, & 11) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: Noise related impacts will be mitigated to a less than significant level. See Sec. Xa.) discussion.

XI. PUBLIC SERVICES. Would the proposal have an effect upon, or result in a need for new or altered government services in any of the following areas:

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Fire protection? (Sources: 1, 3, 6, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Police Protection? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Schools? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Maintenance of public facilities, including roads? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e) Other governmental services? (Sources: 1,3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion: a.-e. The project applicant will be required to pay development impact fees as established by the city per AB 1600 to mitigate impacts to public services.

XII. UTILITIES AND SERVICE SYSTEMS. Would the proposal result in a need for new systems or supplies, or substantial alterations to the following utilities:

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Power or natural gas? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Communication systems? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Local or regional water treatment or distribution facilities? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Sewer or septic tanks? (Sources: 1, 3, 7, & 8) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e) Storm water drainage? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| f) Solid waste disposal? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| g) Local or regional water supplies? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion: a.-g. The project will not result in the need for new systems or supplies, or result in substantial alterations to utilities and service systems.

XIII. AESTHETICS. Would the proposal:

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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- a) Affect a scenic vista or scenic highway? (Sources: 1, 3, & 7)

Discussion: The project is not located in a scenic vista or scenic highway area.

- b) Have a demonstrable negative aesthetic effect? (Sources: 1, 3, & 7)

Discussion: The project is proposed to be designed with high quality materials and architectural design that is suitable to the site and will complement the downtown area, and will not have a demonstrable negative aesthetic effect. The Development Review Committee reviewed and approved the project on October 15, 2007 with the following recommendations:

- 1. Any equipment such as back flow devices, transformers and HVAC equipment shall be full screened from the streets.*
- 2. The final landscape plan along with the final colors and materials will need to go back to the DRC for approval.*

- c) Create light or glare? (Sources: 1, 3, 7, & 8)

Discussion: All light fixtures will be shielded and downcast as required per city regulations.

XIV. CULTURAL RESOURCES. Would the proposal:

- a) Disturb paleontological resources? (Sources: 1, 3, & 7)
- b) Disturb archaeological resources? (Sources: 1, 3, & 7)

Discussion: a.-b. The project site is not located in an area with know paleontological or archaeological resources. If these types of resources are found during grading and excavation, appropriate procedures will be followed including halting activities and contacting the County Coroner, and follow standard mitigation procedures.

- c) Affect historical resources? (Sources: 1, 3, & 7)

Discussion: There are no existing historical resources on the project site.

- d) Have the potential to cause a physical change which would affect unique ethnic cultural values? (Sources: 1, 3, & 7)

Discussion: The project is not proposed in a location where it could affect unique ethnic cultural values.

- e) Restrict existing religious or sacred uses within the potential impact area? (Sources: 1, 3, & 7)

Discussion: Discussion: There are no known religious or sacred uses on or near the project site.

10 Environmental Checklist Form

ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less Than Significant Impact	No Impact
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XV.RECREATION. Would the proposal:

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Increase the demand for neighborhood or regional parks or other recreational facilities? (Sources: 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The project will not significantly affect the demand for parks and recreational facilities. The project complies with the build-out scenario of the General Plan which has adopted park and recreation facility thresholds per population.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| b) Affect existing recreational opportunities? (Sources 1, 3, & 7) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The project will not affect existing recreational opportunities.

MANDATORY FINDINGS OF SIGNIFICANCE.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? (Sources: 1 & 3) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The project does not have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory.

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| Does the project have the potential to achieve short-term, to the disadvantage of long-term environmental goals? (Sources: 1 & 3) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The project will not likely have a potential to achieve short-term, to the disadvantage of long-term environmental goals.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.) (Sources: 1 & 3) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The project will not result in significant cumulative impacts.

- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly? (Sources: 1 & 3) | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|

Discussion: The project will not result in substantial adverse environmental impacts on human beings, either directly or

10 Environmental Checklist Form

		Potentially Significant		
ISSUES (and Supporting Information Sources):	Potentially Significant Impact	Unless Mitigation Incorporated	Less Than Significant Impact	No Impact

indirectly.

11. EARLIER ANALYSIS AND BACKGROUND MATERIALS

Earlier analyses may be used where, pursuant to tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or negative declaration. Section 15063 (c)(3)(D). The earlier documents that have been used in this Initial Study are listed below.

Reference Number	Document Title	Available for Review At
1	City of Paso Robles General Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
2	Seismic Safety Element for City of Paso Robles	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
3	Final Environmental Impact Report City of Paso Robles General Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
4	Soil Survey of San Luis Obispo County, California Paso Robles Area	USDA-NRCS, 65 Main Street-Suite 108 Templeton, CA 93465
5	California Building Code	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
6	City of Paso Robles Standard Conditions of Approval For New Development	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
7	City of Paso Robles Zoning Code	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
8	City of Paso Robles, Water Master Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
9	City of Paso Robles, Sewer Master Plan	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
10	Federal Emergency Management Agency Flood Insurance Rate Map	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446
11	Noise Study Report	City of Paso Robles Community Development Department 1000 Spring Street, Paso Robles, CA 93446

Attachments:

- Attachment 1 – Site Plan
- Attachment 2 – Floor Plans
- Attachment 3 – Existing Site Plan
- Attachment 4 – Preliminary Grading Plan
- Attachment 5 – Elevations
- Attachment 6 – Noise Study Report
- Attachment 7 – Rainmaker III Envi- R/O
- Attachment 8 – Environmentalist II-A

Air Quality Mitigation Measures

AQ-1: **Prior to issuance of a construction permit**, the following measures shall be incorporated into the construction phase of the project and shown on all applicable plans prior to construction permit issuance:

- a. Reduce the amount of the disturbed area where possible.
- b. Use of water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site. Increased watering frequency would be required whenever wind speeds exceed 15 mph. Reclaimed (nonpotable) water should be used whenever possible.
- c. All dirt stock-pile areas should be sprayed daily as needed.
- d. All roadways, driveways, sidewalks, etc. to be paved should be completed as soon as possible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used.
- e. Building pads should be laid as soon as possible after grading unless seeding or soil binders are used.

AQ-2: **Prior to issuance of a construction permit**, *the subject project shall be in compliance with the SLO Co. Air Pollution Control District and the San Luis Obispo County Clean Air Plan standard and discretionary conditions of approval.*

Noise Mitigation Measures

N-1: Prior to issuance of construction permits, the applicant shall submit revised plans showing the use of Ryko bifold doors at the entrance and exit of the car wash facility. The applicant shall submit documentation in the form of a noise diagram from the manufacturer verifying the statements made by Mike McGinness, which support the operational mitigation measure.

N-2: The applicant shall limit hours of car wash operation from 7 a.m. to 7 p.m. to avoid Community Noise Equivalency weighting factors.

Cultural Resources Mitigation Measures

CR-1: In the event archaeological resources are unearthed or discovered during any construction activities, the following standards apply:

- a. Construction activities shall cease, and the Community Development Director shall be notified so that the extent and location of discovered materials may be recorded by a qualified archaeologist, and disposition of artifacts may be accomplished in accordance with state and federal law.
- b. In the event archaeological resources are found to include human remains, or in any other case where human remains are discovered during construction, the County Coroner is to be notified in addition to the Community Development Director so that proper disposition may be accomplished.

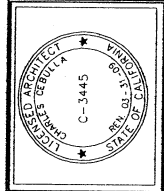
Water Mitigation Measures

W-1: Water discharge from this project will be required to meet all Municipal codes, CBC codes, and the National Pollution Discharge Elimination System (NPDES) requirements relating to water discharge.



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 TEL: (805) 733-1111
 FAX: (805) 733-0203
 E-MAIL: cebullagroup@earthlink.net
 ARCHITECT: CHARLES CEBULLA
 DESIGNER: KATIE CEBULLA
 DESIGNER: KEN CEBULLA

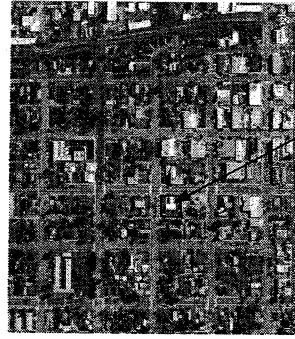
PROJECT:
 NEWS SERVICE STATION
 CONVENIENCE STORE
 141 SPRING STREET
 PASO ROBLES, CA 93444
OWNER:
 STEVE BLAYNEY
 141 SPRING STREET
 PASO ROBLES, CA 93444
 (805) 238-1415



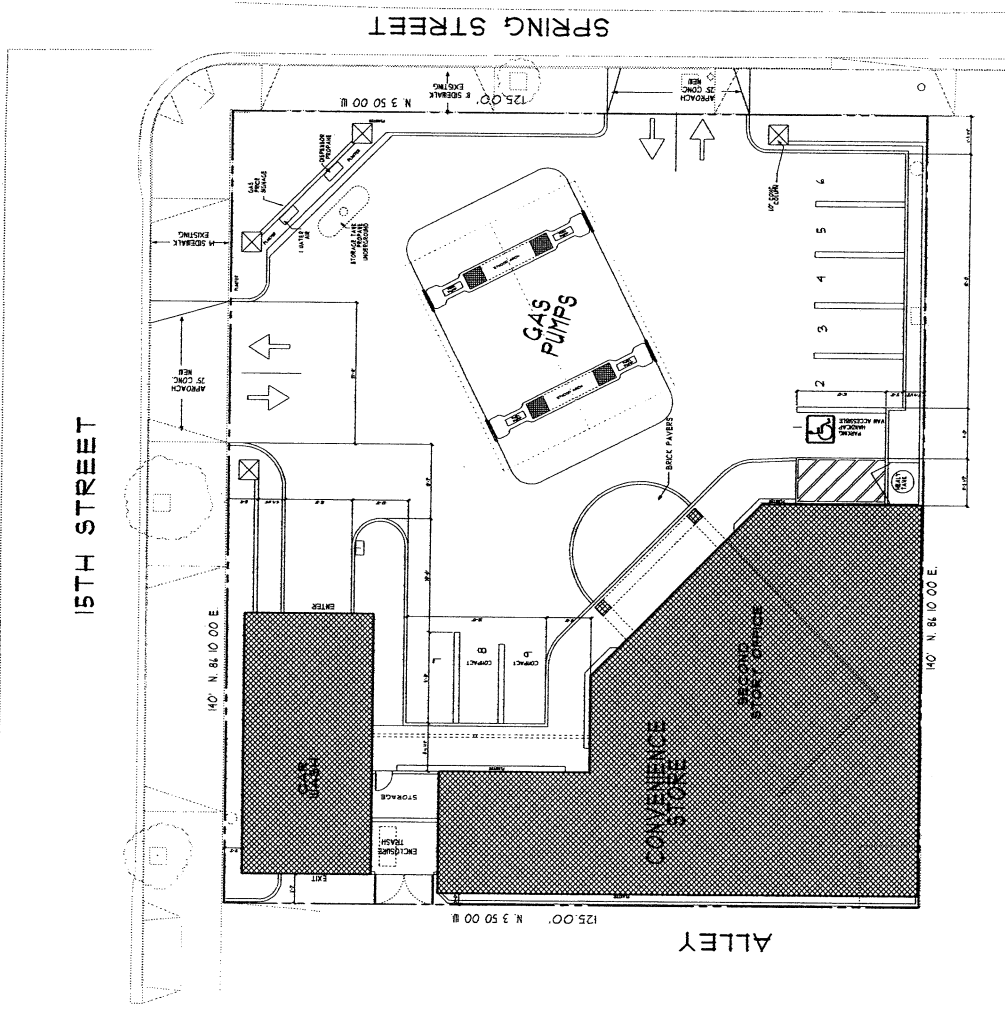
REVISIONS

JOB # 05111
 DATE: 8-2-07
SHEET NO
 1

PROJECT DATA
 ADDRESS 141 SPRING STREET
 LOT SIZE = 11500 SQ. FT.
 BUILDING COVERAGE = 4155.5 SQ. FT.
 PAVED AREA = 10250 SQ. FT.
 LANDSCAPE AREA = 2214.5 SQ. FT. 13%
 PARKING PROVIDED =
 1- HANDICAP
 1- 5'x8'
 2- 8'x11' COMPACT
 BUILDING AREA 5
 CONVENIENCE STORE = 4228 SQ. FT.
 STORAGE = 108 SQ. FT.
 TRASH = 111 SQ. FT.
 CAR WASH = 510 SQ. FT.



SITE
 VICINITY MAP



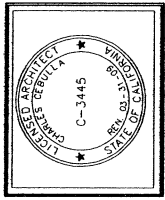
N
SITE PLAN
 SCALE 1" = 10'-0"

Attachment 1 - Site Plan



CEBULLA ASSOCIATES
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 15150 BUCHANAN
 PASO ROBLES, CA 93448
 FAX (805) 413-8263
 Architect: CHARLES CEBULLA
 Designer: TRITT CEBULLA

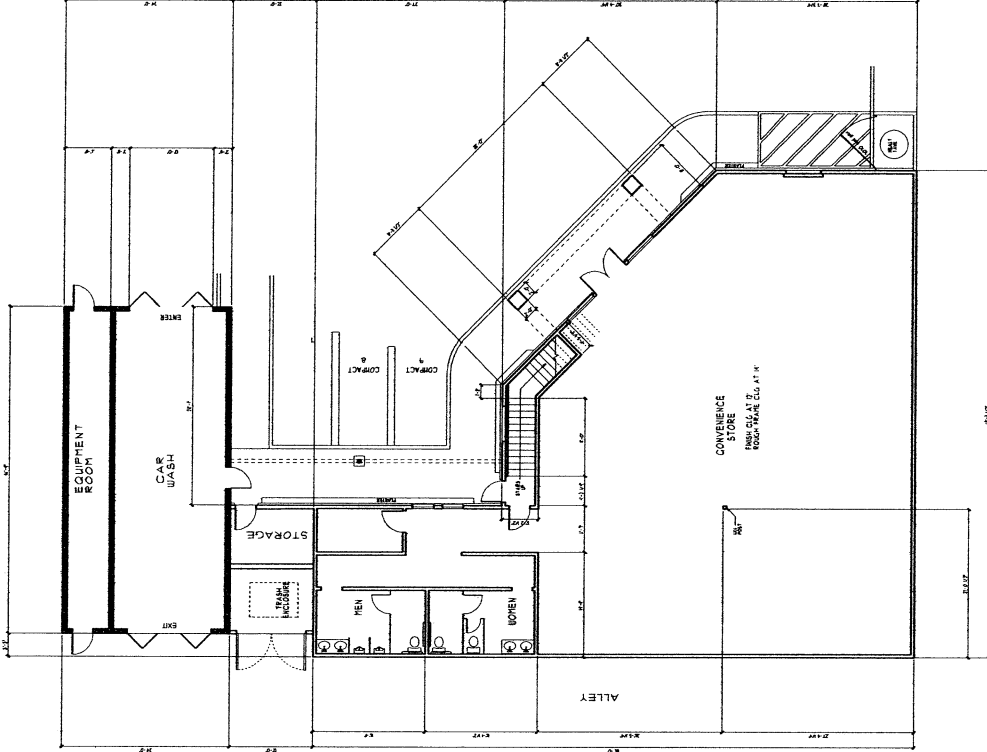
PROJECT:
 NEW SERVICE STATION
 SERVICE BAYS STORE
 SERVICE BAYS STORE
 144 SPRING STREET
 PASO ROBLES, CA 93444
OWNER:
 WERE BLANDES
 144 SPRING STREET
 PASO ROBLES, CA 93444
 (805) 239-4415



REVISIONS:

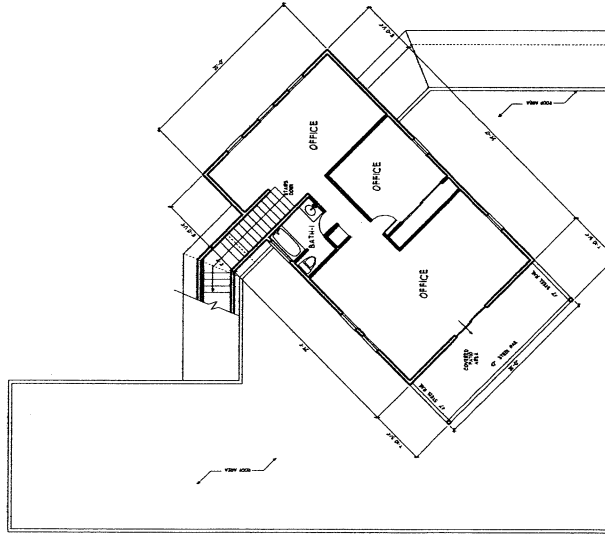
FLOOR PLAN

JOB #: 05111
 DATE: 4-2-08
SHEET NO
 2



FIRST FLOOR PLAN

SCALE 1/8" = 1'-0"



SECOND FLOOR PLAN

SCALE 1/8" = 1'-0"

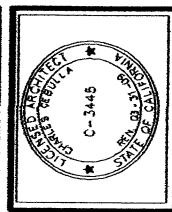
Attachment 2 - Floor Plans



CEBULLA ASSOCIATES
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 DANNY MATT CEBULLA
 DANNY RON CEBULLA

PROJECT:
 NEW SERVICE STATION
 SERVICE BAY STORE
 141 SPRING STREET
 PISMO ROBLES, CA 93444

OWNER:
 STEVE ELBAARD
 141 SPRING STREET
 PISMO ROBLES, CA 93444
 (805) 338-1441

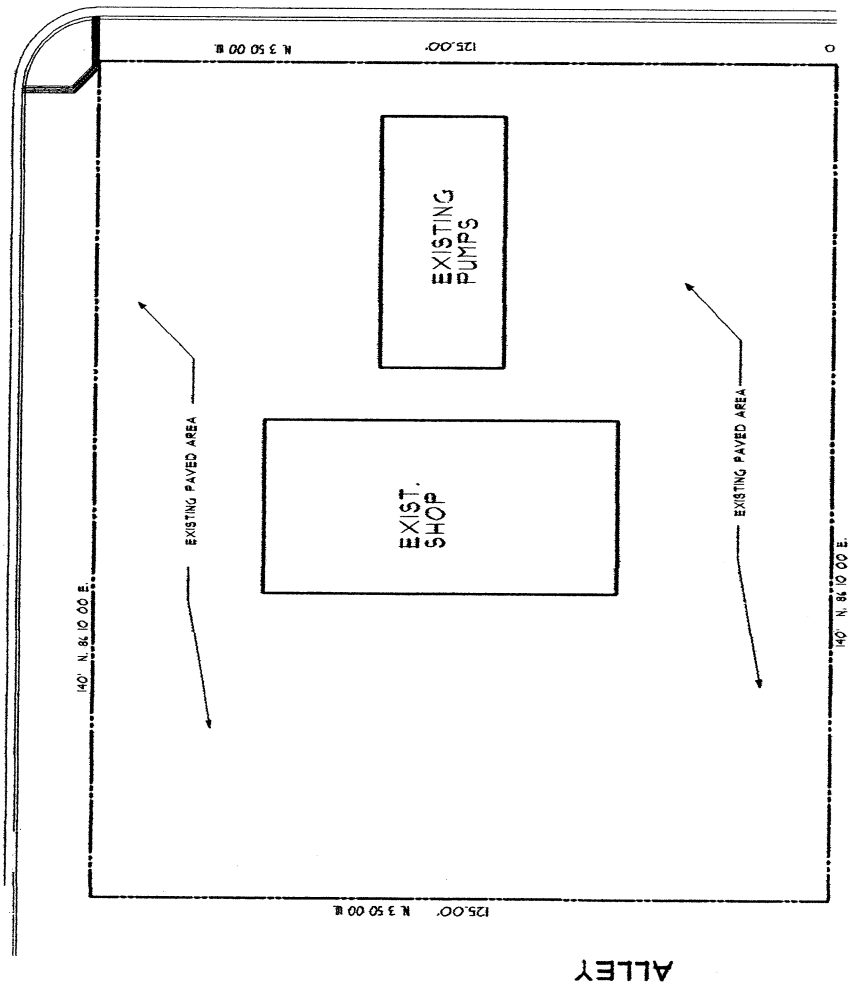


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JOB # 06111
 DATE: 8-2-07
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2

15TH STREET

SPRING STREET

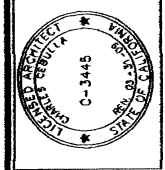


Attachment 3 - Existing Site Plan



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 DESIGNER: CHARLES CEBULLA
 DESIGNER: RON CEBULLA

PROJECT:
 TRANSIT STATION I
 CONVENIENCE STORE
 141 SPRING STREET
 PLEASANTON, CA 94566
OWNER:
 BAY AREA RAILROAD
 141 SPRING STREET
 PLEASANTON, CA 94566
 (925) 399-1418

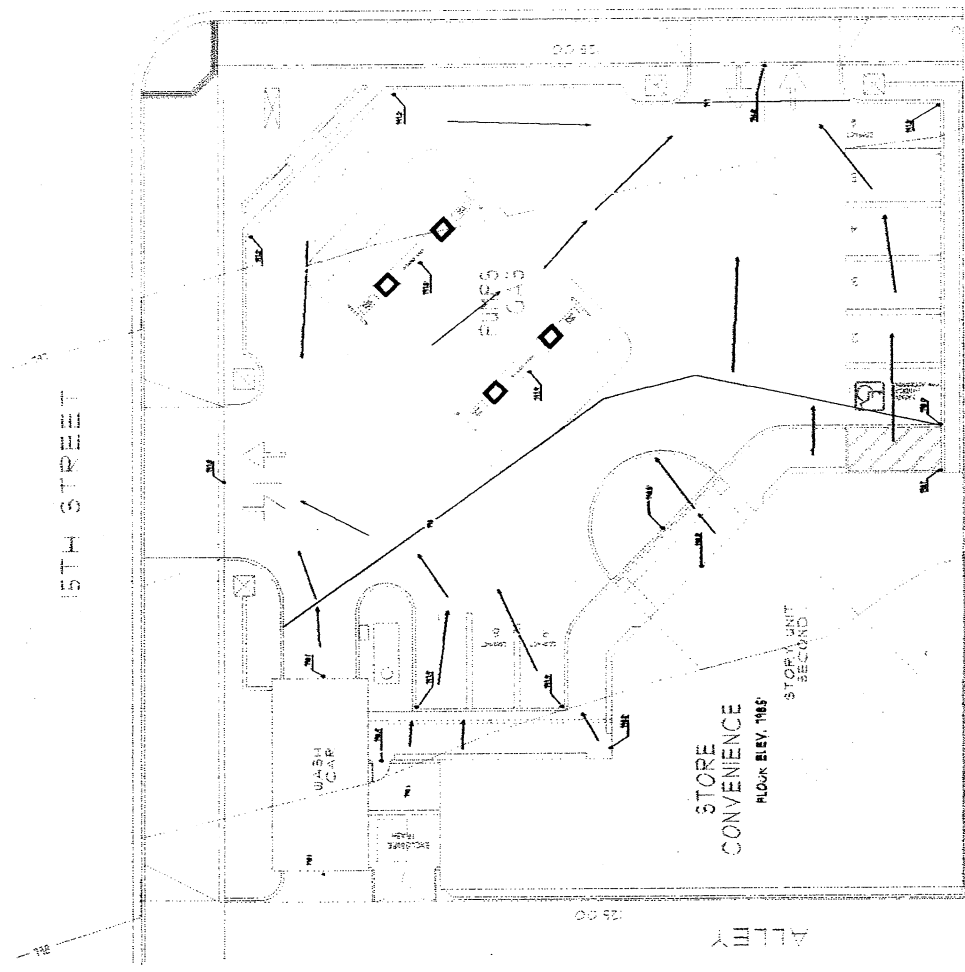


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JOB #: 05117
 DATE: 8-2-05
SHEET NO
3

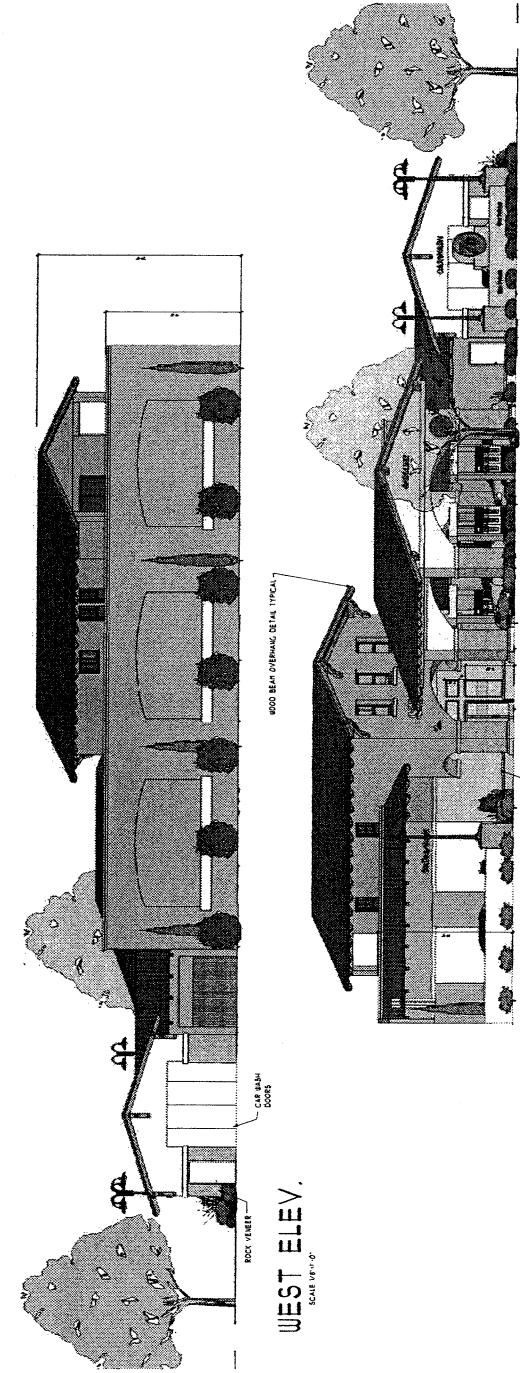
- LOADING NOTES:**
- APPROXIMATE BARTH QUANTITIES, CUT 50 CU-YD
 FILL 50 CU-YD.
 - CONTRACTOR TO VERIFY EXISTING UTILITIES
 BEFORE SITE AND VERIFY LOCATING THE LOCATION OF
 CONCRETE FOUNDATIONS PRIOR TO
 CALLING FOR CITY INSPECTION.
 - ALL FOUNDATIONS TO BE CONFORMING WITH
 HAVING JURISDICTION AND RECOMMENDATIONS
 OF THE SUPERVISING SOILS ENGINEER.
 - CONTRACTOR TO VERIFY ALL UTILITIES
 ALL PUBLIC AND PRIVATE PROPERTY ON OR
 ADJACENT TO THE SITE.
 - ALL CUT AND FILL SLIPES SHALL NOT EXCEED
 A GRADE OF 3 HORIZONTAL TO 1 VERTICAL.
 - ALL AREAS TO RECEIVE FILL OR STRUCTURES
 SHALL BE SOAKED TO A DEPTH OF ONE FOOT
 AND WATERED AND COMPACTED AS REQUIRED.
 - ALL FILL AREAS SHALL BE WATERED AND COMPACTED
 PRIOR TO FINAL INSPECTION.
 - URING CONSTRUCTION AND UNTIL LANDSCAPE IS IN PLACE
 CONTROL OR AS INSTRUCTED BY BUILDING INSPECTOR
 A COMPACTION TEST REPORT PRIOR TO PLACEMENT OF
 CONCRETE.

FILED TO 07 00 00 00



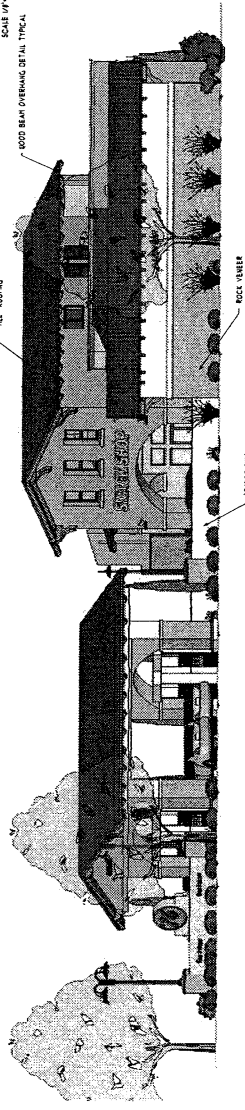
PRELIMINARY GRADING
 SCALE 1"=40'

Attachment 4 - Preliminary Grading Plan



WEST ELEV.
SCALE 1/8"=1'-0"

EAST ELEV.
SCALE 1/8"=1'-0"



NORTH ELEV.
SCALE 1/8"=1'-0"

SOUTH ELEV.
SCALE 1/8"=1'-0"

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WWW: www.cebullassoc.com
OWNER: STEVE ELSTATED
DESIGNER: PATT CEBULLA
DRAWN: RON CEBULLA

PROJECT:
NEW SERVICE STATION
NEW SERVICE STORE
SERVICE BAYS
SERVICE STREET
PASO ROBLES, CA 93446

OWNER:
STEVE ELSTATED
15146
PASO ROBLES, CA 93446
805.739-1445



REVISIONS

JOB # 15111
DATE 8-2-01
SHEET NO
3

Paso Robles

FEB 29 2008

Planning Division

**Cebulla Car Wash
Paso Robles, California**

NOISE STUDY REPORT

Prepared for:

Mr. Matt Cebulla
Cebulla Associates
P.O. Box 42
Pismo Beach, CA 93448

Prepared by:



A DIVISION OF
SWCA
ENVIRONMENTAL CONSULTANTS

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TABLE OF CONTENTS

- I. Introduction 1
- II. Applicable Noise Standards 1
 - A. Environmental Setting 1
 - B. Stationary Noise Sources 1
- III. Project Information..... 6
- IV. Stationary Noise Assessment 7
 - A. Methodology..... 7
 - B. Existing Noise Levels 7
 - C. Future Noise Levels at the Site 8
 - D. Compliance with City Standards 8
- V. Noise Impacts and Mitigation Measures..... 9
 - A. Impacts 9
 - B. Mitigation Measures 9
- VI. References 11

TABLES

- Table 1: Land Use Compatibility for New Development near Transportation Sources 6
- Table 2: Measured Noise Levels..... 7
- Table 3: Expected Noise Levels of Ryko XS with On-Board Dryer 8

FIGURES

- Figure 1: Vicinity Map 3
- Figure 2: Location Map..... 4
- Figure 3: Noise Data Collection Location 8

APPENDICES

Appendix A: Land Use Map and Proposed Project Plans

Appendix B: Noise Study Background Information

--Manufacturer Noise Diagram

--Noise Compatibility Index

--Instrumentation and Terminology for Noise Investigations

I. INTRODUCTION

This noise study report has been prepared for the proposed construction of a car wash (proposed project) located at 1441 Spring Street in Paso Robles, California (refer to Figures 1 and 2). Vanessa Amerson, acoustical consultant with Morro Group, a division of SWCA, has prepared this report at the request of Mr. Matt Cebulla of Cebulla and Associates (applicant). Mr. Cebulla enclosed a letter from Darren Nash, Associate Planner with the City of Paso Robles (City) citing applicable goals and policies of the City's Noise Element for Conditional Use Permit 07-019. This report has been prepared to address the potential noise-related impacts resulting from the proposed project for submittal to the City during the permitting phase of the proposed project.

II. APPLICABLE NOISE STANDARDS

A. ENVIRONMENTAL SETTING

The project site is within an existing gas station on the southwest corner of Spring Street and 15th Street between a commercial business and a drive-through bank across the street. Spring Street is one of the major arterials serving the City. An alley runs parallel to Spring Street at the rear of the gas station. The residential neighborhood behind the gas station includes non-residential uses and associated parking lots. The topography surrounding the project site is characterized as a "hard" site, which means that it reflects sound rather than absorbs sound. Hard sites have a reflective ground surface such as concrete pavement.

The proposed project site is currently zoned Highway Commercial (C-2). The site is within the Community Commercial land use category with a Mixed Use overlay, within Office Professional and Historic Preservation overlays (refer to Appendix A, Land Use Map). Adjacent parcels to the north, south, and east are zoned C-2 and are within the Community Commercial land use category. The existing residential land use area to the west is the primary noise-sensitive land-use in the nearby vicinity of the proposed project site.

B. STATIONARY NOISE SOURCES

Noise standards applicable to the proposed project are outlined in the Noise Element of the City of Paso Robles General Plan. Stationary noise sources generated by the proposed project may affect sensitive land uses within the existing residential multi-family (RMF-8) land use designation located to the west of the proposed project site (refer to Figure 3). Stationary noise sources are any fixed or mobile source not exempted from local control by existing federal, state, or local regulations. Examples of such sources include industrial and commercial facilities and vehicle movements on private property, such as parking lots. Stationary noise sources that would result from the proposed car wash (e.g., idling cars, rotating scrubbers, dryers, etc.) would potentially result in a stationary noise impact to sensitive noise receptors in the area. The City takes into account the time of day that the noise will occur because the community's perception of noise is relative to normal daily activity. Community Noise Equivalent Level (CNEL) weights evening noise (7 p.m. to 10 p.m.) by a factor of 5, and nighttime levels (10 p.m. to 7 a.m.) by a factor of 10. For example, 65 decibels would be judged as 70 decibels at 8 p.m. and would be judged as 75 decibels at 11 p.m.

The City of El Paso de Robles General Plan 2003 Noise Element includes the following goals, policies, and action items related to noise and this type of proposed development:

Goal N-1: Minimize exposure to noise and generation of noise.

POLICY N-1A: Noise Minimization. New development shall be designed to comply with the maximum, allowable Noise Exposures of 65 dB CNEL for outdoor activities (except for parks); and 45 dB CNEL for indoor activities. Noise measurement (dB Ldn or CNEL) is calculated using a daily average that takes into account the time of day the noise occurs. Sounds occurring at night are weighted more heavily.

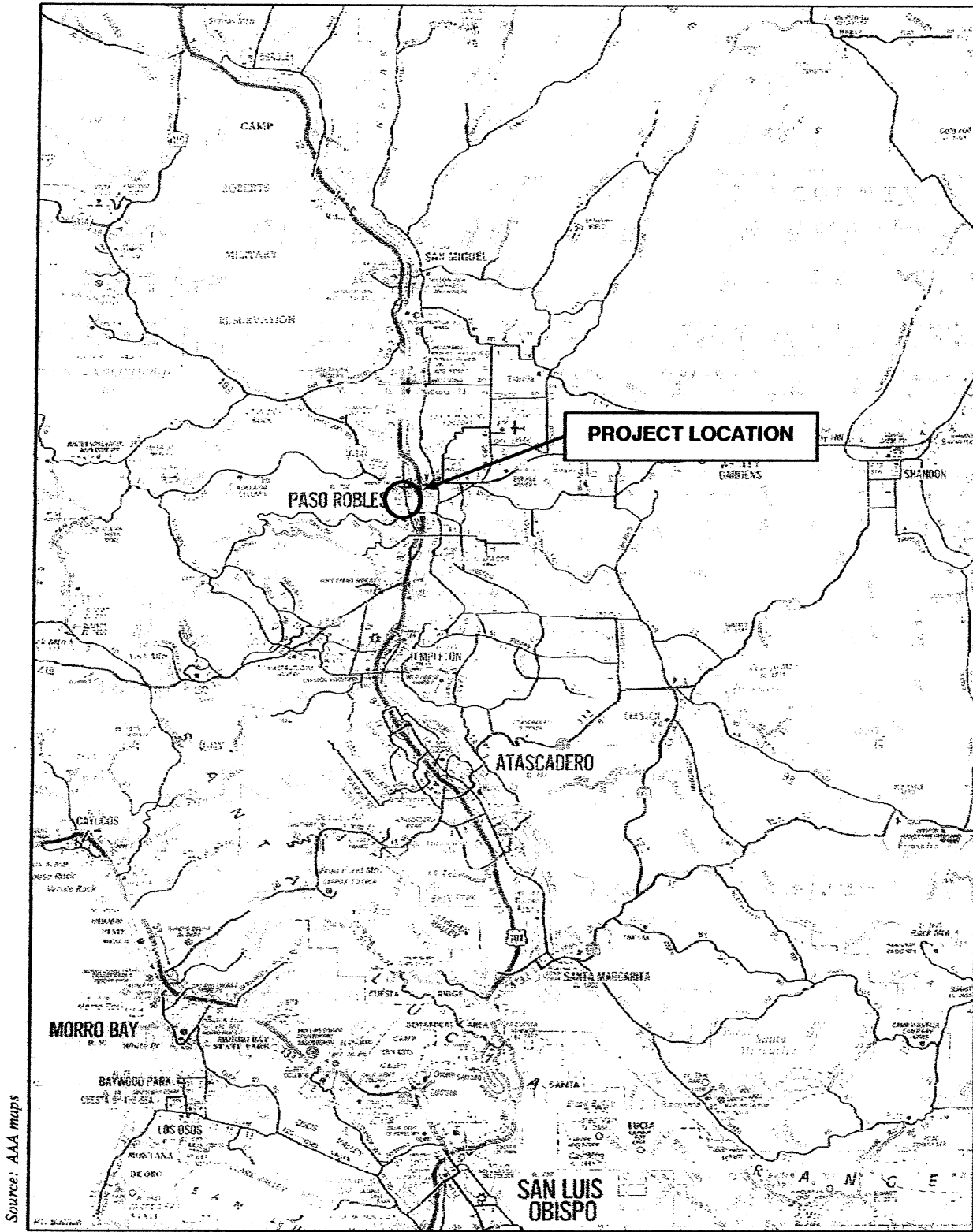
Action Item 7. When mitigation must be applied to satisfy City noise standards, the following priorities for mitigation shall be observed, where feasible:

- First:** Use of setbacks and/or open space separation;
- Second:** Site layout/orientation/shielding of noise-sensitive uses with non-noise-sensitive uses;
- Third:** Structural measures: acoustical treatment of buildings and noise barriers constructed of concrete, wood or materials other than earth;
- Fourth:** Construction of earthen berms or soundwalls.

Action Item 11. For non-residential noise-sensitive uses, mitigation may proceed in a manner similar to that described in Table N-6 of the Noise Element as included in the Noise Element Technical Appendix. Where an acoustical analysis prepared by a noise expert is called for, the report of said analysis should be submitted prior to approval of a subdivision map, parcel map, conditional use permit, or development plan so that decision makers can determine if Noise Element policies and standards are being properly implemented.

Action Item 13. Permit new development only where the noise level due to existing stationary noise sources will not exceed the noise level standards of Table N-5 of the Noise Element or where noise mitigation measures have been incorporated into the design of the development to reduce noise exposure to or below the levels specified in Table N-5 of the Noise Element.

Action Item 14. Where a new stationary noise source is proposed to be developed or an existing stationary noise source is proposed to be expanded, mitigation of noise levels that exceed those listed in Table N-5 of the Noise Element shall be required. This program does not apply to noise levels associated with agricultural operations.



Source: AAA maps

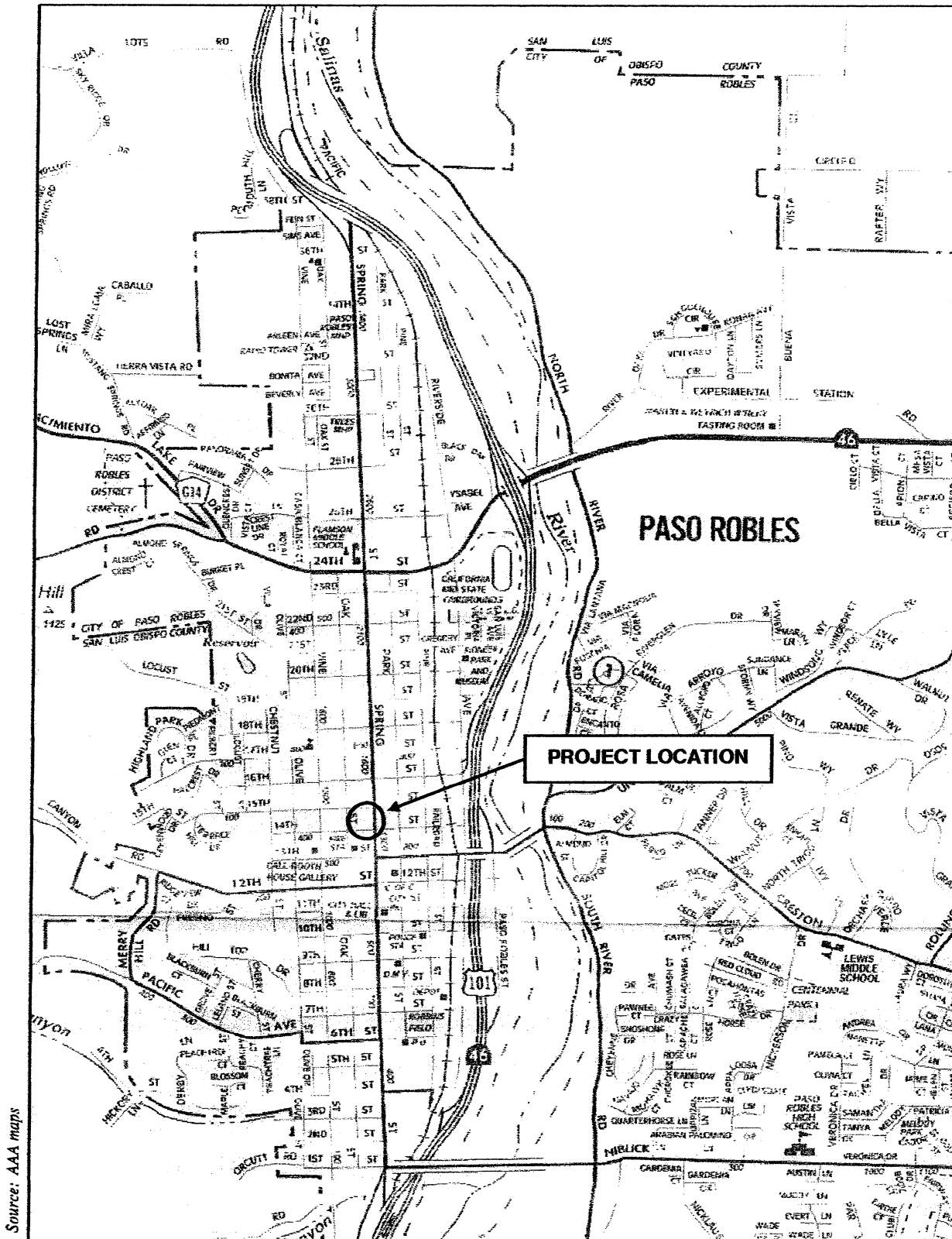


NORTH

Not to Scale

**Project Vicinity Map
FIGURE 1**

Morro Group – a division of SWCA



Source: AAA maps

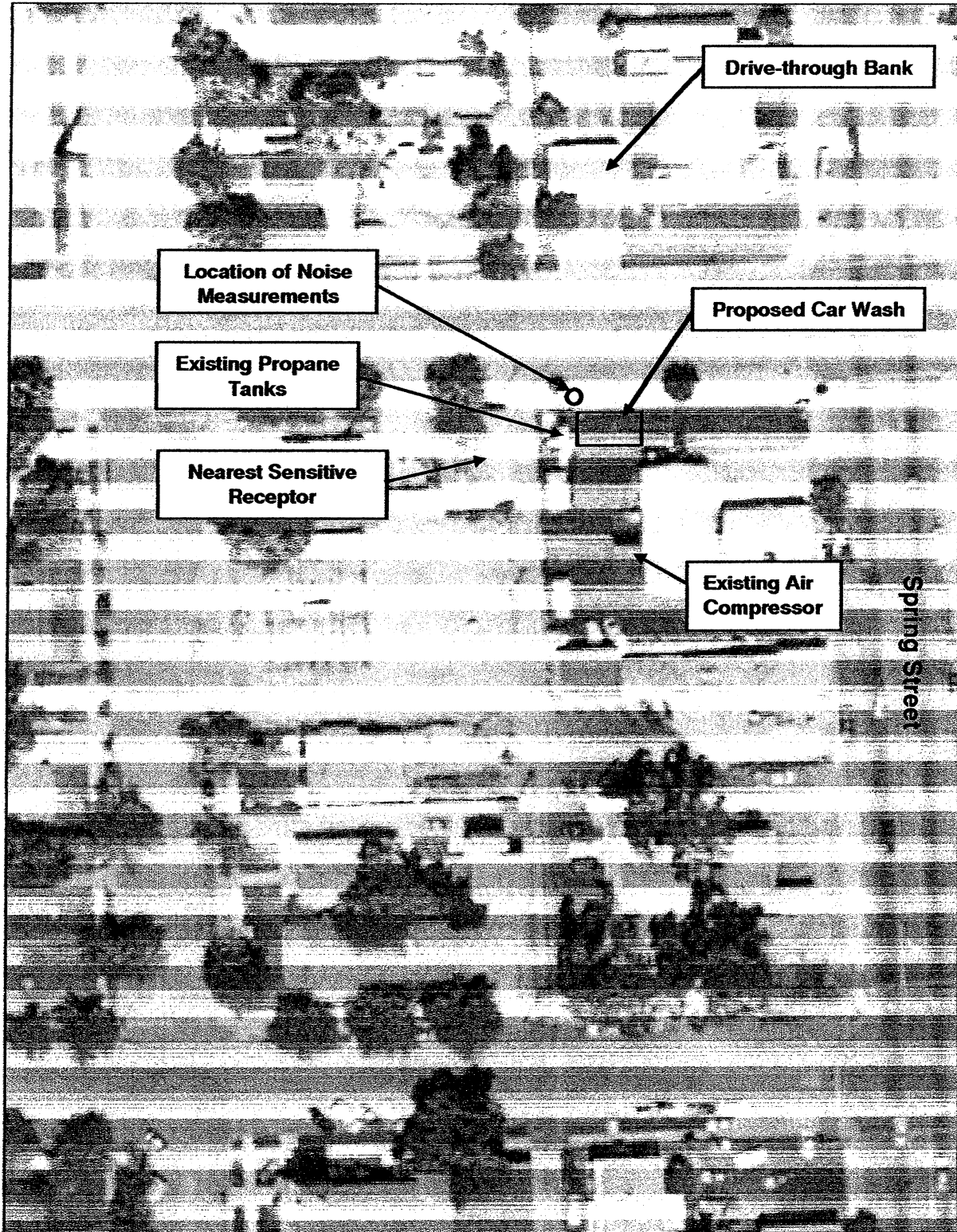


NORTH

Not to Scale

Project Location Map

FIGURE 2



NORTH
Not to Scale

Noise Measurement Location Map
FIGURE 3

III. PROJECT INFORMATION

Currently, the gas station includes a fueling station, enclosed service station, propane filling station and a convenience store. The applicant provided information relating to the proposed plans on January 8, 2008. The proposed project includes: reconstructing the convenience store to the southwest corner of the site bordering the adjacent commercial use and the alley; altering the location of the gas pumps, propane dispenser, and air compressor; and, constructing a car wash (refer to Appendix A, Proposed Project Plans). The proposed car wash would be located at the northwest corner bordering the alley and 15th street (refer to Figure 3). The applicant also provided the manufacturer’s noise literature specific to the proposed car wash model, Ryko XS, which includes an on board dryer (refer to Appendix B, Manufacturer Noise Diagram).

The nearest sensitive receptor is a converted garage residential unit located to the west of the project, approximately 40 feet from the exit of the proposed car wash facility. There is approximately 20 feet between the property line of the project site and the residential property line; the converted garage is located within a few feet of the property line. The next nearest sensitive receptors are residences are approximately 200 feet away to the northwest. Both sensitive receptors are within the residential multifamily zoning and have a threshold of 65 CNEL with a conditionally acceptable threshold of 70 CNEL (refer to Table 1). The drive-through bank located across 15th Street to the north, the adjacent commercial use to the south, and businesses across Spring Street to the east have a noise threshold of 70 CNEL.

**TABLE 1
Noise Compatibility Index**

Land Use	Exterior Noise Exposure, Ldn or CNEL (dB)						
	55	60	65	70	75	80	85
Residential Single Family	[Shaded area from 55 to 70]						
	[Shaded area from 60 to 75]						
	[Shaded area from 70 to 85]						
Residential Multi-Family	[Shaded area from 55 to 70]						
	[Shaded area from 60 to 75]						
	[Shaded area from 70 to 85]						
Commercial	[Shaded area from 55 to 70]						
	[Shaded area from 60 to 75]						
	[Shaded area from 70 to 85]						
	Normally Acceptable						
	Conditionally Acceptable						
	Normally Unacceptable						
	Clearly Unacceptable						

Source: Paso Robles Noise Element, Policy Document

IV. STATIONARY NOISE ASSESMENT

A. METHODOLOGY

The standard procedure for assessing new stationary noise effects includes measuring the ambient pre-project noise level at a select location, measuring the distance from the proposed sound source to the nearest sensitive receiver from the sound source, and comparing the ambient noise readings to expected noise levels from operation of the noise source. The expected noise levels are then compared to published threshold values in the City's Noise Element to determine if a significant change in the noise environment would occur and if an exceedance of the threshold value would be expected. In this case, direct noise measurements are available and can be applied to the site and the nearest sensitive receptor.

B. EXISTING NOISE LEVELS

Ambient noise levels at the location of the proposed project were measured from 4:00 p.m. to 5:00 p.m. on the afternoon of January 25, 2008 (refer to Table 2). Approximate ten-minute duration measurements were taken for each situation. The maximum and minimum one-second noise levels are recorded by the noise meter, and are included for informational purposes. Further analysis is based on integrated average noise levels (Leq). Ambient noise measurements were made at the boundary of the proposed project site and the alley, adjacent to the nearest sensitive receptor site (refer to Figure 3). Existing stationary noise levels in the project area originate from a variety of operations occurring in and around the project area, e.g. propane fueling station, air compressor and idle cars at the gas station and the drive through bank. Ambient noise levels of approximately 52 to 60 dBA were measured at the proposed project site. Measured noise levels are summarized in Table 2 below.

TABLE 2
Measured Noise Levels

Location*	Min dBA	Max dBA	Leq dBA
Ambient Noise	51.6	71.3	59.8
Ambient Noise with Propane Tanks	56.3	80.0	67.1
Ambient Noise with Air Compressor	55.5	83.4	66.9

* Measurement locations are shown on Figure 3.

Descriptions of each noise measurement are as follows:

- Situation 1 – Noise from vehicular traffic on Spring Street and idling cars in gas station.
- Situation 2 – The same noise from situation 1 with the propane tanks in use.
- Situation 3 – The same noise from situation 1 with the air compressor in use.

C. FUTURE NOISE LEVELS AT THE SITE

Based on the information provided by the applicant, operation of the car wash would generate a noise level up to 87 dBA, as measured from 10 feet from the car wash exit. Noise would attenuate as follows: 84 dBA at 20 feet and 80 dBA at 30 feet. In order to confirm the accuracy of the projection, Morro Group conducted a study at the nearest car with the same model car wash with an on board dryer. The referenced car wash is located at the Shell Gas Station on the corner of Madonna and Los Osos Valley Road, in the City of San Luis Obispo. According to this study the noise level was 87 dBA at 10 feet, 79 dBA at 20 feet and 76 dBA at 30 feet (refer to Table 3 below).

**TABLE 3
Expected Noise Levels of Ryko XS with On-Board Dryer**

Source	dBA at 10 feet	dBA at 20 feet	dBA at 30 feet
Noise Diagram supplied by applicant	87.2	84.2	80.6
Study at nearest same model car wash	87.4	79.5	75.7

Since the area proposed for the car wash (84 dBA) is currently being utilized as a propane fueling station (67 dBA), future noise levels surrounding the immediate project area can be expected to increase significantly over ambient conditions.

D. COMPLIANCE WITH CITY STANDARDS

According to the Noise Element’s Noise Compatibility Index, 65 dBA is normally acceptable for multi-family residential noise exposure from stationary sources and 70 dBA is conditionally acceptable (refer to Appendix B). The car wash as proposed would generate 87 dBA at 10 feet away from the source. The current ambient noise level is 67 dBA during operation of the air compressor or the propane tanks at 10 feet away from the proposed source. The applicant proposes to relocate other noise sources, the air compressor and propane tanks. The relocation negates the need to add the other noise sources to the car wash noise because of the noise attenuation over distance and the noise buffering caused by the car wash facility (refer to Table 2). For the purpose of calculation, the 60 dBA ambient noise level applies because the relocation of the air compressor and the propane tanks. When predicting noise levels that differ by 10 or more decibels, the larger decibel measurement is considered the prediction. In this case the equation would combine 60 dBA (proposed project without car wash) with 87 dBA (car wash) to arrive at the prediction of 87 dBA. Implementation of the proposed project would increase the ambient noise level by 20 dBA as measured from 10 feet away from the source. This noise level would exceed the acceptable threshold by 22 dBA.

V. NOISE IMPACTS AND MITIGATION MEASURES

A. IMPACTS

The proposed project would not comply with the goals set forth by the City because operation of the car wash would exceed the 70 dB threshold for the Community Commercial Land Use Category by 17 dB. Also, exposure to the operation of the car wash would exceed the 65 dB threshold for the Residential Multi-Family Land Use Category by 19 dB.

B. MITIGATION MEASURES

In general, noise can be mitigated physically with changes to the location of the source, topography, or complimentary structures like sound walls. Noise can also be mitigated operationally with changes to the technology of the source. Physical options for on-site noise mitigation are limited. For example, sound barriers would restrict traffic flow and there is not enough space on the 17,000-square foot site for significant sound attenuation over distance. Due to the close proximity of the residential sensitive receptor to the west and the hard, reflective nature of the site and surrounding uses, the noise generated by the car wash would not be mitigated by naturally occurring sound attenuation such as topography.

However, there is the opportunity to significantly reduce the noise impact operationally by modifying the source itself while maintaining the proposed project’s layout and circulation pattern. According to Mike McGinness, a sales representative with Ryko, if Ryko Bifold Doors are installed at the entrance and exit of the facility, the noise would be reduced to 65 dBA measured 10 feet from the source and approximately 60 dBA at 20 feet from the source. The doors are closed during operation and only open upon completion of all washing and drying cycles. In order to arrive at a mitigated project noise prediction, the ambient noise of the site is added to the manufacturer’s noise levels, assuming the addition of the doors. Combining 65 dBA (mitigated car wash) to 60 dBA (ambient noise) results in a noise level of 66 dBA at 10 feet away from the noise source. The mitigated car wash project would comply with the standards and policies set forth by the City of Paso Robles. The mitigated proposed project noise level, measured 10 feet from the source, at 66 dBA would not be considered a significant change over the site’s current noise level at 67 dBA; the human ear can only detect noise level changes of 3 dBA or more. Operation of the proposed project would not exceed the 65 dB threshold for residential outdoor use or 70 dB for commercial use. The following formula applies to find the predicted noise level of the mitigated proposed project at 10 feet from the source:

Combining Sound: $L_{EQ} = 10 \text{ Log}_{10} [(10^{\frac{SPL(1)}{10}} + 10^{\frac{SPL(2)}{10}} + 10^{\frac{SPL(3)}{10}} + \dots + 10^{\frac{SPL(N)}{10}})];$

yields: $L_{EQ} = 10 \text{ Log}_{10} [(10^{\frac{65}{10}} + 10^{\frac{65}{10}} + 10^{\frac{65}{10}} + 10^{\frac{65}{10}} + 10^{\frac{65}{10}} + 10^{\frac{65}{10}})] = 66.1 = 66 \text{ dBA}$

Per the discussion in section II.B. above, when the mitigated proposed project Community Noise Equivalency Level is 65 dB it will be judged as 70 dB after 7 p.m. and 75 dB after 10 p.m. These levels exceed the thresholds for the residential multi-family zoning.

The two recommended mitigation measures are:

1. Prior to issuance of construction permits, the applicant shall submit revised plans showing the use of Ryko bifold doors at the entrance and exit of the car wash facility. The applicant shall submit documentation in the form of a noise diagram from the manufacturer verifying the statements made by Mike McGinness, which support the operational mitigation measure.
2. The applicant shall limit hours of car wash operation from 7 a.m. to 7 p.m. to avoid Community Noise Equivalency weighting factors.

Based on implementation of these, Noise Element standards for stationary sources would not be exceeded and significant noise impacts would not be experienced by the adjacent existing and future land uses or the nearest sensitive residential receptor.

VI. REFERENCES

Bruel & Kjaer, Environmental Noise. 2000.

City of El Paso de Robles General Plan, Noise Element. 2003.

Environmental Program: Noise, Air Quality, and Hazardous Waste Management Office
Technical Noise Supplement. 1998b. Sacramento, CA

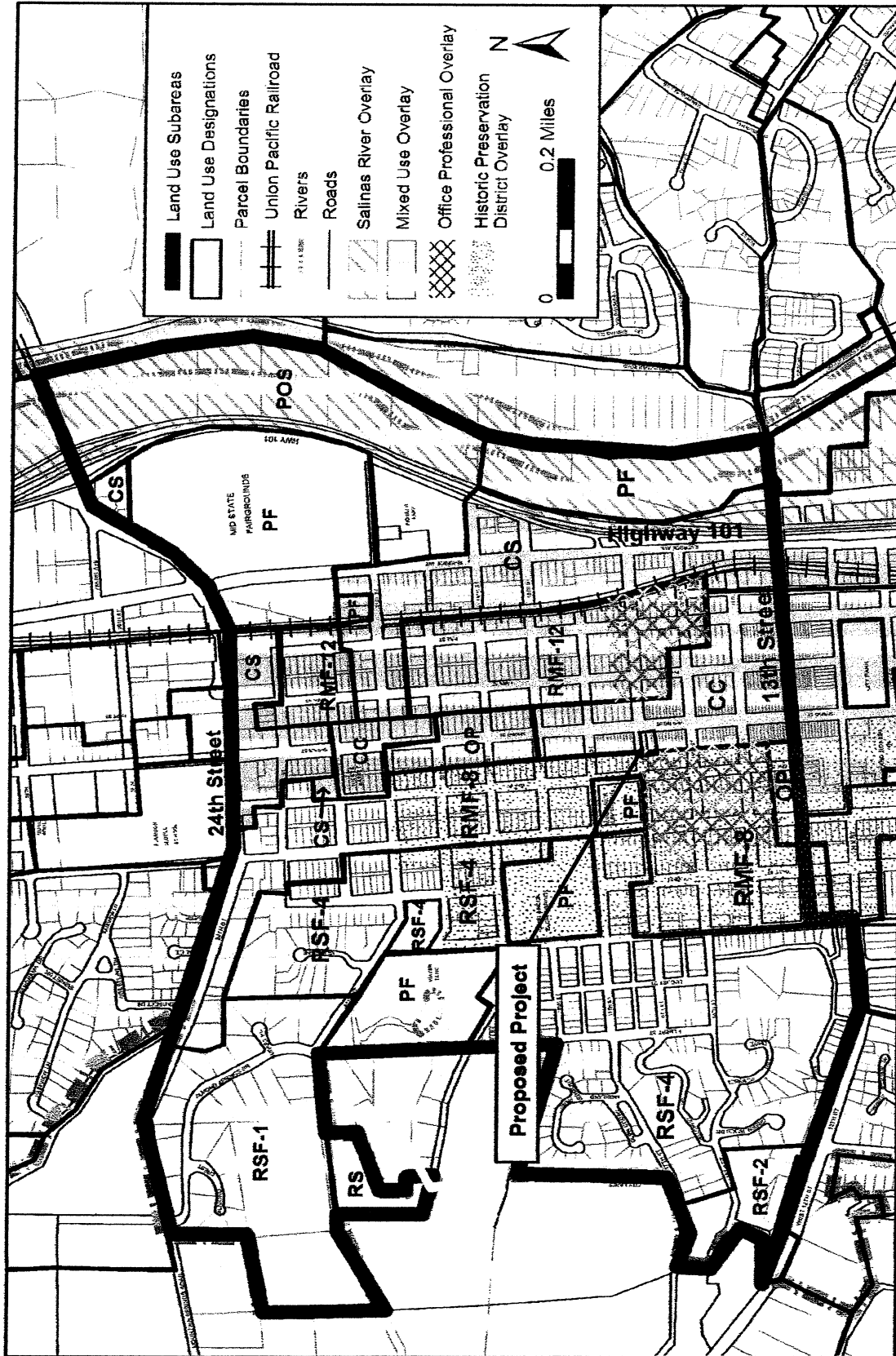
Federal Transit Administration 1995. *Transit Noise and Vibration Impact Assessment*.
(DOT-T-9516.)

Federal Highway Administration. 1995. *Highway Traffic Noise Analysis and Abatement Policy and Guidance*. Office of Environment and Planning. Noise and Air Quality Branch. Washington, DC.

McGinness, Mike, Ryko Manufacturing Representative. Pers. Conv., January 28, 2008 and February 12, 2008.

APPENDIX A

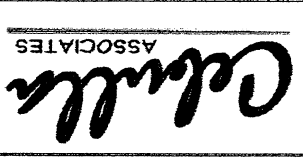
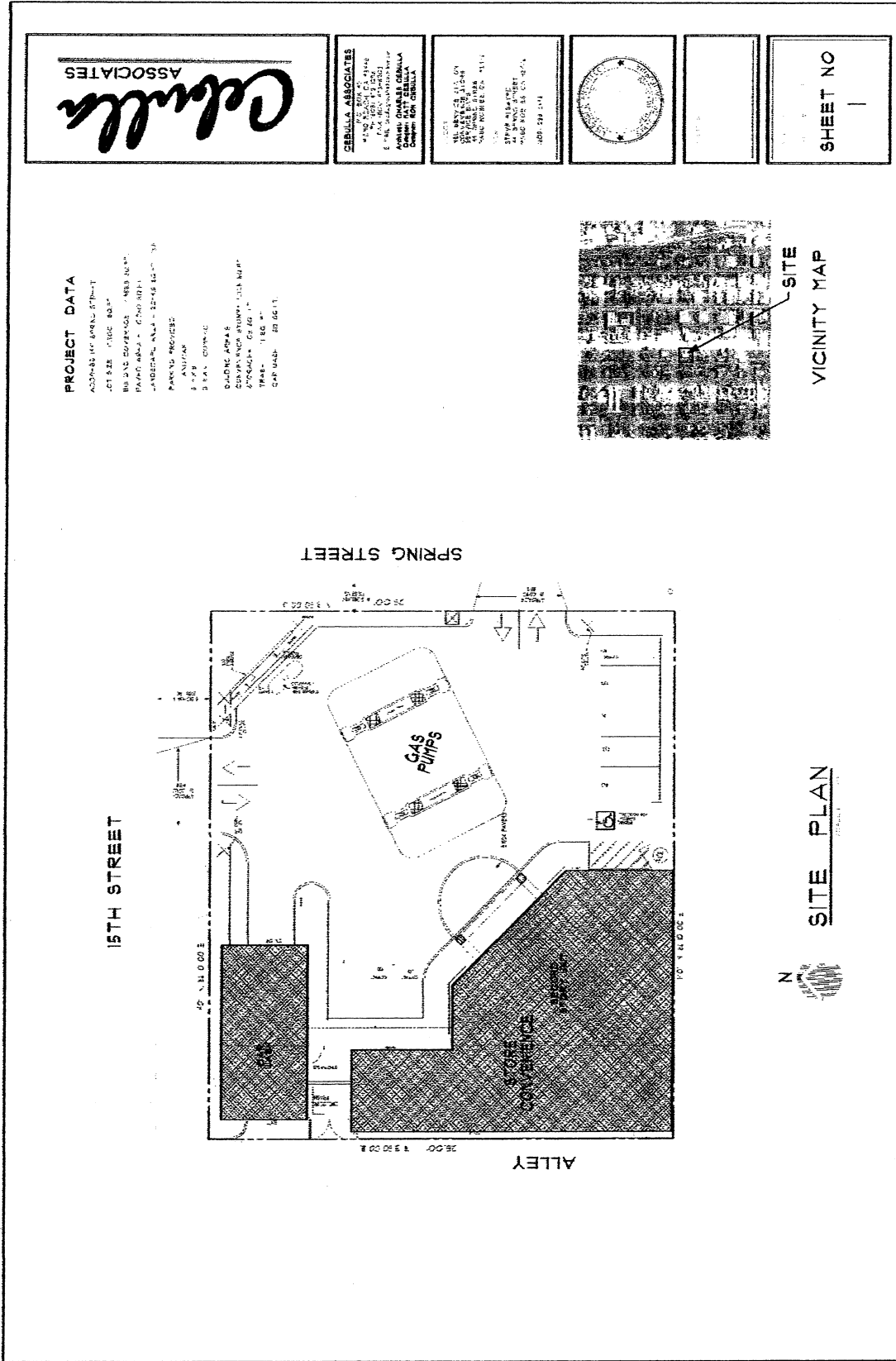
- **Land Use Map**
- **Proposed Project Plans**



Source: City of El Paso de Robles 2003 General Plan

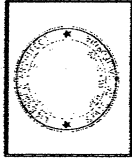
Land Use Map

Morro Group - a division of SWCA

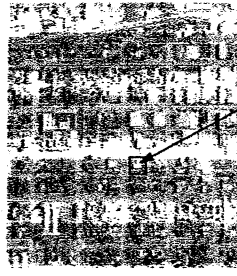


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NOISE STUDY
 FOR THE
 PROPOSED
 CAR WASH
 AT
 401 SPRING STREET
 ALBANY, NY
 JOB NO. 100-111



SHEET NO
 1



SITE PLAN

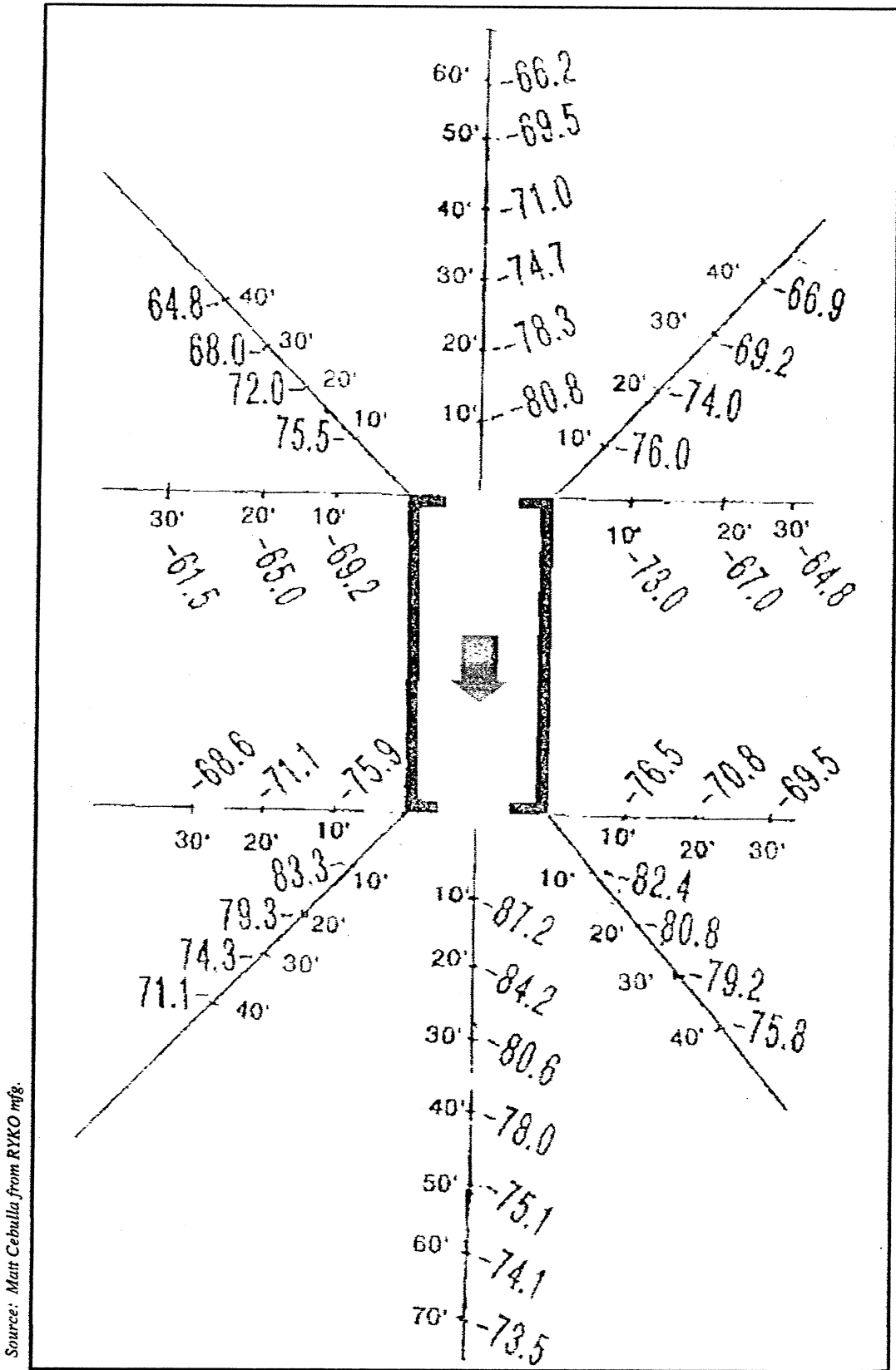
Proposed Project Plans

Source: Cebulla and Associates

Marro Group - a division of SWCA

APPENDIX B

- **Noise Study Background Information**
 - Manufacturer Noise Diagram
 - Noise Compatibility Index
 - Instrumentation and Terminology for Noise Investigations



Source: Matt Cebulla from RYKO mfg.

Manufacturer Noise Diagram

LAND USE CATEGORY	COMMUNITY NOISE EXPOSURE						
	Ldn or CNEL, dBA						
	55	60	65	70	75	80	85
RESIDENTIAL - LOW DENSITY SINGLE FAMILY, DUPLEX, MOBILE HOMES		Normally Acceptable	Normally Acceptable	Normally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable
RESIDENTIAL - MULTI-FAMILY			Conditionally Acceptable	Conditionally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable
TRANSIENT LODGING - MOTELS, HOTELS			Conditionally Acceptable	Conditionally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable
SCHOOLS, LIBRARIES, CHURCHES, HOSPITALS, NURSING HOMES			Conditionally Acceptable	Conditionally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable
AUDITORIUMS, CONCERT HALLS, AMPHITHEATRES	Conditionally Acceptable	Conditionally Acceptable	Conditionally Acceptable	Conditionally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable
SPORTS ARENA, OUTDOOR SPECTATOR SPORTS	Conditionally Acceptable	Conditionally Acceptable	Conditionally Acceptable	Conditionally Acceptable	Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable
PLAYGROUNDS, NEIGHBORHOOD PARKS				Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable	Clearly Unacceptable
GOLF COURSES, RIDING STABLES, WATER RECREATION, CEMETERIES					Normally Unacceptable	Clearly Unacceptable	Clearly Unacceptable
OFFICE BUILDINGS, BUSINESS COMMERCIAL AND PROFESSIONAL				Conditionally Acceptable	Conditionally Acceptable	Normally Unacceptable	Clearly Unacceptable
INDUSTRIAL, MANUFACTURING, UTILITIES, AGRICULTURE					Conditionally Acceptable	Clearly Unacceptable	Clearly Unacceptable

Normally Acceptable
Specified land use is satisfactory, based upon the assumption that any buildings involved are of normal conventional construction, without any special noise insulation requirements

Normally Unacceptable
New construction or development should generally be discouraged. If new construction or development does proceed, a detailed analysis of the noise reduction requirements must be made and needed noise insulation features included in the design

Conditionally Acceptable
New construction or development should be undertaken only after a detailed analysis of the noise reduction requirements is made and needed noise insulation features included in the design. Conventional construction, but with closed windows and fresh air supply systems or air conditioning will normally suffice

Clearly Unacceptable
New construction or development should generally not be undertaken.

Source: Guidelines for the Preparation and Content of Noise Elements of the General Plan, California Office of Planning and Research, 1998

Source: Paso Robles Noise Element, Policy Document

Noise Compatibility Index

INSTRUMENTATION AND TERMINOLOGY FOR NOISE INVESTIGATIONS

INSTRUMENTATION

The subject noise investigation has been conducted using a Bruel and Kjaer (B & K) Model 2230 precision integrating sound level meter calibrated externally at the beginning and end of each period of measurement using a B & K Model 4230 acoustic calibrator. In combination, these instruments yield sound level measurements accurate to within 0.1 decibel (dB). The Model 2230 fulfills standards of relevant sections of IEC (International Electrotechnical Commission) 651 and ANSI (American National Standard) S1.4.1971 for Type 1 (precision) integrating sound level meters.

The microprocessor of the Model 2230 computes and stores/displays the following measurements:

The sound pressure level (SPL) is updated once each second on the digital display at a resolution of 0.1 dB, and 64 times per second on the analog display at a resolution of 2 dB. The mechanism of averaging levels during the display interval may be "fast" or "slow". The setting is normally "fast", as this is required for Leq and SEL discussed below.

The sound equivalent level (Leq) is the average sound pressure level for the period of measurement based on equal energy. The meter internally computes a new Leq from the SPL (RMS) and updates the digital display once each second. The measurement period is limited only by battery life which is approximately 8 hours. This parameter is used primarily to describe environmental noise.

The sound exposure level (SEL) is the constant level which if maintained for one second would have the same acoustic energy as the total noise for the period of measurement. This parameter is used primarily in determining the noise exposure in unusually noisy working environments or for measuring specific events such as an individual aircraft flyover or a train passage.

The maximum (Max.) and minimum (Min.) sound pressure levels during the period of measurement are updated once each second from the RMS average sound pressure level. For periods of measurement in the range of 1 to 10 minutes, these values are reasonable approximations of the sound pressure level exceeded 1% of the time and 99% of the time, respectively.

All of the above can be measured using frequency weightings of the "A" or "C" scales in accordance with IEC 651, or a "linear" (20 Hz to 20 kHz) or "all pass" (10 Hz to 50 kHz) filter settings. The "A" scale is weighted to most closely approximate the response of an average human ear, and is the setting most used in conducting measurements of environmental noise.

TERMINOLOGY

Noise, as used herein, is defined as unwanted sound. However, because the instruments that detect the small changes in atmospheric pressure that are perceived as sound cannot distinguish between that which is wanted (e.g., birds singing, waves on a beach, etc.) and that which is not (e.g., traffic noise), measurements of "noise" are more accurately described as measurements of sound pressure.

Changes in sound pressure normally experienced in the human environment extend across a very large range. The sound pressures in an average room are in the range 1,000 times the sound pressure at the threshold of hearing, and the sound pressure of a large truck is approximately 100,000 times that threshold. Because of this large range, it is convenient to describe sound in terms of its energy level with respect to that of the threshold of hearing. This method of description is called the decibel scale (dB). In mathematical terms, the sound pressure level, SPL = $10 \text{ Log } (p/p_0)^2$ dB, where p_0 is the sound pressure at the threshold of hearing (20 microPascals). In practical terms, it is adequate to note that the decibel scale is logarithmic (like the Richter scale for earthquakes), that it conveniently compresses the numbers involved from a range of 20-200,000,000 to a range of 0-130, and that it is oriented to human response in that an increase of approximately 10 dB is normally perceived as a doubling of the sound level.

In recent years, various methods and "scales" have been devised to describe noise in the human environment. These methods have had two basic objectives: 1) to represent a physical condition that is constantly changing over a wide range of values by a single numerical descriptor; and 2), to adjust that descriptor in a way that most reasonably reflects the degree of annoyance of the varying noise levels.

1. Statistical Descriptors

Statistical descriptors most often used to describe variations in noise level include:

- L₉₀ The level exceeded 90% of the time during a specified period, usually 1 hour, 24 hours, or during the day or the night. In some instances, this value may be considered the background level.
- L₅₀ The level exceeded 50% of the time during a specified period as noted above. This value has sometimes been considered the average or median noise level.
- L₁₀ The level exceeded 10% of the time during a specified period as noted above. For traffic noise, this value has been considered the peak period level.
- L₁ The level exceeded 1% of the time during a specified period as noted above. This value may be considered the peak noise level.

The most significant drawback to the use of these descriptors, particularly L₅₀ as representing an average, is that they do not take into account the logarithmic nature of the decibel scale and the relatively higher energy content of higher decibel levels. That is, the average energy content of

50 dB and 60 dB for equal periods of time is not 55 dB, but rather 57.4 dB (i.e., the log of the average of the antilogs).

A parameter that more accurately describes average noise is the Equivalent Continuous Sound Level (Leq), which is the continuous sound level having the same energy content as the varying level for the period of measurement. Prior to the availability of microprocessors at reasonable cost, the hand-computation of Leq from a series of individual measurements was a tedious task. However, meters are now available that internally compute Leq, continuously as with the Model 2230 discussed above, or for a specified period usually one minute. Because of this technical advance, measurements of Leq for various periods of time have become the basic parameter in evaluating environmental noise.

2. Weighted Noise Levels

Because the same level of noise is more annoying to people if it occurs at night, scales have been devised that weight nighttime noise at a higher level than daytime noise. The scales most commonly in use are:

CNEL Community Noise Equivalent Level weights evening noise (7 p.m. to 10 p.m.) by a factor of 5, and nighttime levels (10 p.m. to 7 a.m.) by a factor of 10. Mathematically, evening levels are increased by 5 dB, and nighttime levels are increased by 10 dB in computing a 24-hour geometric average.

Ldn Day-Night Equivalent Level is similar to CNEL but it does not include a weighting factor for evening noise levels.

Of the above, CNEL came into use first, and it is the standard in regulating noise levels in the vicinity of airports. Ldn is a simplification of CNEL, and is more commonly used in regulating land use where traffic noise is a potential problem. These levels apply for a minimum period of 24 hours, but may be applied for periods as long as one year. The difference may be significant where noise levels are near regulatory limits, and where there are seasonal or weekly variations in a noise source of concern.

3. Practical Applications

From a practical standpoint, the Ldn noise level is essentially equivalent to the peak-hour noise level for most situations involving noise from vehicular traffic, and the peak-hour Leq can be used as the Ldn level, avoiding the costs of 24 hours of measurement.



RAINMAKER III ENVI-R/O

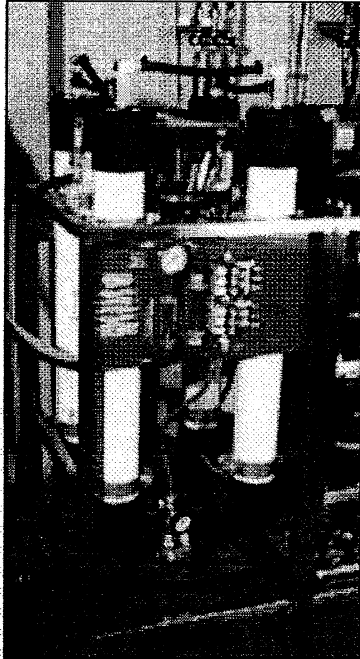
REVERSE OSMOSIS SPOT-FREE RINSE

NO WATER SOFTENER NEEDED!

Attachment 7 - Rainmaker III Envi- R/O

RAINMAKER III ENVI-R/O

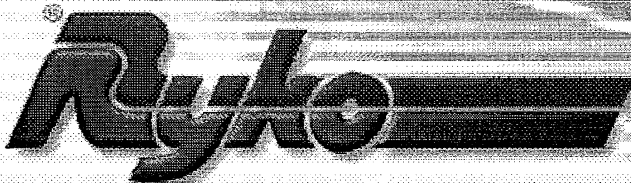
REVERSE OSMOSIS, SPOT-FREE RINSE



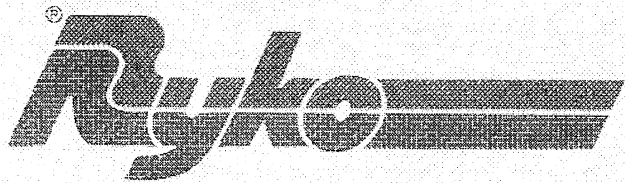
The revolutionary RYKO Rainmaker III ENVI-R/O system will provide your site with pure spot-free rinse water **WITHOUT** the use of a water softener! The ENVI-R/O system lowers operating costs by eliminating the need for water softener salt and maintenance, yet still delivers the spot-free rinse results that customers demand.

BENEFITS OF THE RAINMAKER III ENVI-R/O

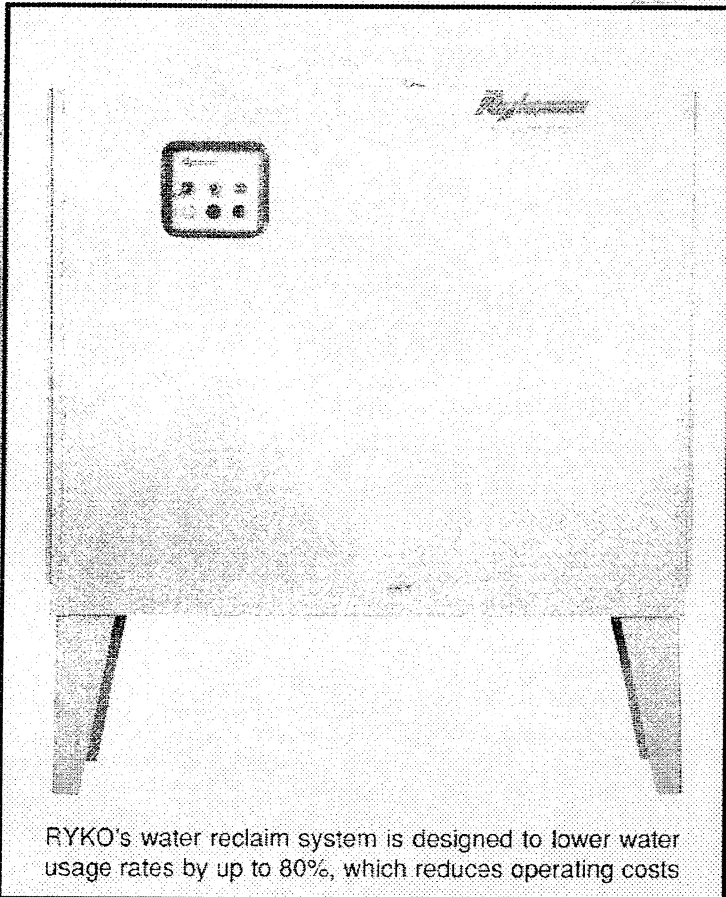
- Perfect for use in areas of the country where regulations prohibit water softeners.
- Chemical Free- no injection of acids or anti-scaling chemicals required
- Reduced Maintenance – Eliminates maintenance costs associated with water softeners
- Reduced Operating Costs – No salt or other water softening agents are required



RYKO Manufacturing Company
11600 NW 54th Avenue – Grimes, IA. 50111
Phone (515) 986-3700 – Fax (515) 986-3621
E-mail sales@ryko.com - www.ryko.com



ENVIRONMENTALIST II-A

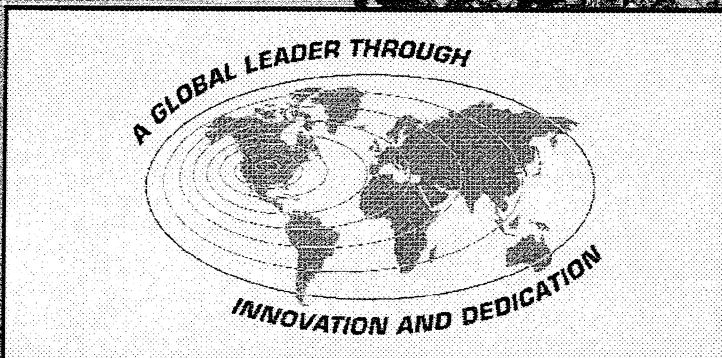


RYKO's water reclaim system is designed to lower water usage rates by up to 80%, which reduces operating costs

If it were not for water, we would not be the nation's leading manufacturer of quality vehicle wash systems. Furthermore, we would not be the leading supplier of car wash equipment to a number of Fortune 500 companies—if it were not for water.

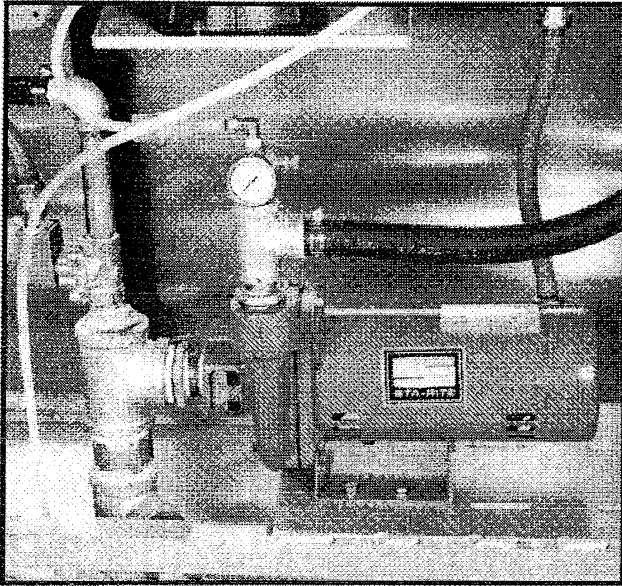
We're RYKO® Manufacturing, an ISO 9001 approved company. Obviously, we recognize the importance of water. More importantly, we recognize the importance of water conservation. In fact, during the past two decades, we've been responsible for conserving and reusing billions of gallons of water at RYKO-equipped locations worldwide. With the new Environmentalist II-A water reclaim system, we're prepared to conserve even more water, and operating costs, in the future.

The Environmentalist II-A water reclaim system is RYKO's latest addition to its fine line of car wash accessories. The EV II-A is designed to reduce operating costs for car wash operators by storing and then re-using water that has been used in previous wash cycles. Furthermore, the water maintains a residual detergent content which helps reduce the cost of chemical usage. Perhaps more importantly, the EV II-A meets civic environmental codes.

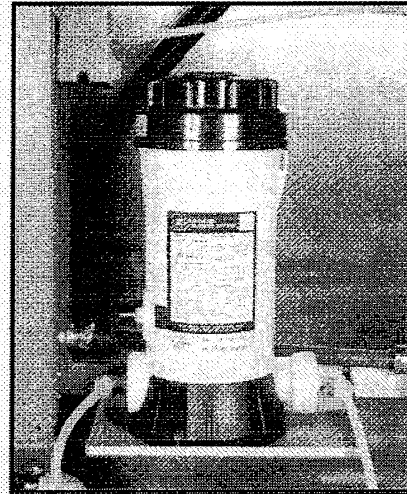


Here's how it works. During a full wash cycle, the water used is drained into two underground tanks where it is held in storage. During subsequent washes, the water stored is sent through a cyclone separator where particulate matter is removed. The water is then used in another wash cycle but is not applied during the rinse cycle where only fresh water is used. The net result is a clean vehicle that attracts and retains car wash customers, while water and detergent usage is reduced by as much as 80%. The bottom line result is a direct decrease in operating costs and an increase in car wash revenues.

The Environmentalist II-A is built from corrosive-resistant aluminum that withstands years of exposure to wet environments. The self-priming pumps are a protective feature which allows the EV II-A to operate continuously with uninterrupted ease. The built-in chlorination system dramatically reduces the level of lingering odor that can result from water storage. The EV II-A is also equipped with programmable controls for reliable and efficient operation. Furthermore, a frost protection option is available to allow the EV II-A to operate even under extreme climatic environments. Recirculation is standard as an aid to reducing bacteria growth.



The high-efficient pumps are precision created with self-priming feature.



The optional chlorination system further assists to eliminate odor-causing bacteria.

EV II-A SPECIFICATIONS

Water Requirements

3/4 inch service line, 40 PSI minimum at 15 GPM flow rate. Maximum pressure is 150 PSI.

Electrical Requirements

200V or 230V, 60HZ, 3 Phase, 15 AMPS 460V, 60HZ, 3 Phase, 7.5 AMPS

Cabinet Dimensions

45 inches high
52 inches wide
12 inches deep

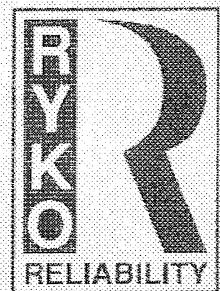
CERTIFIED
ISO-9001



WE'RE WASHING OVER A MILLION VEHICLES A DAY!
RELIABLE • PROVEN • ACCEPTED



RYKO Manufacturing Company
11600 NW 54th Ave. • Grimes, IA 50111
(515) 986-3700 • FAX (515) 986-3621



1/98 5M
17981-000

RESOLUTION NO: _____

**A RESOLUTION OF THE PLANNING COMMISSION
OF THE CITY OF EL PASO DE ROBLES
DENYING CONDITIONAL USE PERMIT 07-019
AT 1441 SPRING STREET
APPLICANT: STEVE ELSAYED
APN: 008-316-016 & -015**

WHEREAS, Section §21.16.200 of the City of El Paso de Robles' Zoning Code requires approval of a Conditional Use Permit for carwashes in the C-2 zone; and

WHEREAS, the applicant, Steve Elsayed, has proposed a Conditional Use Permit to establish an automated carwash service at 1441 Spring Street; and

WHEREAS, during October 15, 2007 the Development Review Committee reviewed Site Plan 07-017 and approved the site plan and design of the expansion of the convince store, but not the carwash; and

WHEREAS, a public hearing was conducted by the Planning Commission on April 22, 2008, to consider the facts as presented in the staff report prepared for this project, and to accept public testimony regarding this Conditional Use Permit; and

WHEREAS, a mitigated negative declaration was prepared for the proposed carwash pursuant to the California Environmental Quality Act (CEQA); and

WHEREAS, based upon the facts and analysis presented in the staff report, public testimony received and subject to the conditions of approval listed below, the Planning Commission makes the following finding:

1. The Planning Commission finds that the establishment, maintenance or operation of the carwash will be detrimental to the health, safety, morals, comfort, convenience and general welfare of the persons residing or working in the vicinity of the proposed use because even with inclusion of mitigation measures the operation of the carwash is not conditional acceptable with the General Plan's Land Use Element due to the proximity of the existing residential land uses and the location of the proposed carwash;
2. The Planning Commission finds that the proposed carwash will not be injurious or detrimental to property and improvements in the area or to the general welfare of the City because it shall comply with all applicable zoning, building and municipal codes;
3. The Planning Commission finds that proposed carwash does not meet the Historic District A's guidelines by not helping to revitalize and enhance the appearance or atmosphere of the downtown. The proposed carwash does not compliment the scale, proportion and tradition of the historic downtown, nor is not pedestrian oriented. The washing and idling of cars does not help improve the atmosphere for the pedestrian;

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission of the City of El Paso de Robles does hereby deny Conditional Use Permit 07-019.

PASSED AND ADOPTED THIS 22nd day of April 2008, by the following roll call vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

ATTEST:

CHAIRMAN ED STEINBECK

RON WHISENAND, PLANNING COMMISSION SECRETARY

RESOLUTION NO: _____

**A RESOLUTION OF THE PLANNING COMMISSION
OF THE CITY OF EL PASO DE ROBLES
APPROVING CONDITIONAL USE PERMIT 07-019
AT 1441 SPRING STREET
APPLICANT: STEVE ELSAYED
APN: 008-316-016 & -015**

WHEREAS, Section §21.16.200 of the City of El Paso de Robles' Zoning Code requires approval of a Conditional Use Permit for carwashes in the C-2 zone; and

WHEREAS, the applicant, Steve Elsayed, has proposed a Conditional Use Permit to establish an automated carwash service at 1441 Spring Street; and

WHEREAS, the proposed use is compatible with the surrounding land uses, and it will not significantly impact traffic congestion along Spring Street and 15th Street provided proper noise attenuation is provided; and

WHEREAS, conditions have been included to control potential impacts resulting from noise and pollution from water drainage; and

WHEREAS, during October 15, 2007 the Development Review Committee reviewed Site Plan 07-017 and approved the site plan and design of the expansion of the convince store, but not the carwash; and

WHEREAS, a public hearing was conducted by the Planning Commission on April 22, 2008, to consider the facts as presented in the staff report prepared for this project, and to accept public testimony regarding this Conditional Use Permit; and

WHEREAS, a mitigated negative declaration was prepared for the proposed carwash pursuant to the California Environmental Quality Act (CEQA); and

WHEREAS, based upon the facts and analysis presented in the staff report, public testimony received and subject to the conditions of approval listed below, the Planning Commission makes the following finding:

1. The Planning Commission finds that the establishment, maintenance or operation of the carwash will not be detrimental to the health, safety, morals, comfort, convenience and general welfare of the persons residing or working in the vicinity of the proposed use because appropriate mitigation measures have been identified to bring the operation of the carwash into compliance with the General Plan's Noise Element.
2. The Planning Commission finds that the proposed carwash will not be injurious or detrimental to property and improvements in the area or to the general welfare of the City because it shall comply with all applicable zoning, building and municipal codes.
3. The Planning Commission finds that proposed carwash meets the Historic District A guidelines by helping to revitalize and enhance the convenience of the downtown by offering a service to the citizens that is limited in the downtown.

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission of the City of El Paso de Robles does hereby approve Conditional Use Permit 07-019 subject to the following conditions:

STANDARD CONDITIONS:

1. The applicant/developer shall comply with those standard conditions which are indicated as applicable in "Exhibit A" to this resolution and all mitigation measures of associated Mitigated Negative Declaration.

SITE SPECIFIC CONDITIONS:

NOTE: In the event of conflict or duplication between standard and site-specific conditions, the site-specific condition shall supersede the standard condition.

2. The project shall be constructed in substantial conformance with the Conditions of Approval established by this Resolution and it shall be constructed in substantial conformance with the following Exhibits:

EXHIBIT	DESCRIPTION
A	Standard Conditions
B	Site Plan
C	Floor Plans
D	Elevations
E	Landscape Plan

3. This Conditional Use Permit (CUP) authorizes the installation of an automated carwash located at 1441 Spring Street.
4. Any condition imposed by the Planning Commission in granting this Conditional Use Permit may be modified or eliminated, or new conditions may be added, provided that the Planning Commission shall first conduct a public hearing in the same manner as required for the granting of the original permit. No such modification shall be made unless the Commission finds that such modification is necessary to protect the public interest and/or neighboring properties, or, in the case of deletion of an existing condition, that such action is necessary to permit reasonable operation and use under the Conditional Use Permit.
5. Prior to commencing the installation of the carwash, the applicants shall obtain Building Permits for any tenant improvements and comply with all required Building and Fire Codes.
6. All business signage (including temporary signs) shall comply with the City Sign Ordinance. A sign permit shall be approved prior to installation of any business signs.

7. Prior to the issuance of the building permits, the applicant shall provide the necessary info to the Public Works Department to insure requirements of the Industrial Waste program are being met. Requirements include but not limited too:
 - The carwash shall have a three stage separator installed for any wastewater discharged to the sewer from the car wash.
 - Any outdoor drains cannot be attached to the sewer unless they are covered and bermed to prevent storm water intrusion to the sewer.
8. Ryko bifold doors shall be installed at the entrance and exit of the car wash facility, and shall also submit documentation in the form of a noise diagram, which support the operational mitigation measure along with a licensed professional's letter verifying the authenticity of the document.
9. Hours of operation of the carwash shall be from 7 a.m. to 7 p.m.
10. Prior to the issuance of a Certificate of Compliance/opening of the business, all of the Departments involved (Building, Fire, Police & Planning) shall ensure that all of the conditions of approval listed in this resolution have been satisfied.

PASSED AND ADOPTED THIS 22nd day of April 2008, by the following roll call vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

ATTEST:

CHAIRMAN ED STEINBECK

RON WHISENAND, PLANNING COMMISSION SECRETARY

EXHIBIT A OF RESOLUTION

**CITY OF EL PASO DE ROBLES STANDARD DEVELOPMENT CONDITIONS
FOR PLANNED DEVELOPMENTS / CONDITIONAL USE PERMITS**

PROJECT #: CUP 07-19

APPROVING BODY: PLANNING COMMISSION

DATE OF APPROVAL: April 22, 2008

APPLICANT: Steve Elsayed

LOCATION: 1441 Spring Street (008-316-016 & -015)

The following conditions that have been checked are standard conditions of approval for the above referenced project. The checked conditions shall be complied with in their entirety before the project can be finalized, unless otherwise specifically indicated. In addition, there may be site specific conditions of approval that apply to this project in the resolution.

COMMUNITY DEVELOPMENT DEPARTMENT - The applicant shall contact the Community Development Department, (805) 237-3970, for compliance with the following conditions:

A. GENERAL CONDITIONS:

- 1. This project approval shall expire on _____ unless a time extension request is filed with the Community Development Department prior to expiration.
- 2. The site shall be developed and maintained in accordance with the approved plans and unless specifically provided for through the Planned Development process shall not waive compliance with any sections of the Zoning Code, all other applicable City Ordinances, and applicable Specific Plans.
- 3. Prior to occupancy, all conditions of approval shall be completed to the satisfaction of the City Engineer and Community Developer Director or his designee.
- 4. Any site specific condition imposed by the Planning Commission in approving this project may be modified or eliminated, or new conditions may be added, provided that the Planning Commission shall first conduct a public hearing in the same manner as required for the approval of this project. No such modification shall be made unless the Commission finds that such modification is necessary to protect the public interest and/or neighboring properties, or, in the case of deletion of an existing

(Adopted by Planning Commission Resolution 00-000)

condition, that such action is necessary to permit reasonable operation and use for this approval.

- 5. This project is subject to the California Environmental Quality Act (CEQA) which requires the applicant submit a filing fee for the Notice of Determination payable to "County of San Luis Obispo". The fee should be submitted to the Community Development Department within 24 hours of project approval which is then forwarded to the San Luis Obispo County Clerk. Please note that the project may be subject to court challenge unless the required fee is paid.
- 6. The site shall be kept in a neat manner at all times and the landscaping shall be continuously maintained in a healthy and thriving condition.
- 7. All signs shall be subject to review and approval as required by Municipal Code Section 21.19 and shall require a separate application and approval prior to installation of any sign.
- 8. All outdoor storage shall be screened from public view by landscaping and walls or fences per Section 21.21.110 of the Municipal Code.
- 9. All trash enclosures shall be constructed of decorative masonry block compatible with the main buildings. Gates shall be view obscuring and constructed of durable materials such as painted metal or chain link with plastic slatting.
- 10. All existing and/or new ground-mounted appurtenances such as air-conditioning condensers, electrical transformers, backflow devices etc., shall be screened from public view through the use of decorative walls and/or landscaping subject to approval by the Community Development Director or his designee. Details shall be included in the building plans.
- 11. All existing and/or new roof appurtenances such as air-conditioning units, grease hoods, etc. shall be screened from public view. The screening shall be architecturally integrated with the building design and constructed of compatible materials to the satisfaction of the Community Development Director or his designee. Details shall be included in the building plans.
- 12. All existing and/or new lighting shall be shielded so as to be directed downward in such a manner as to not create off-site glare or adversely impact adjacent properties. The style, location and height of the lighting fixtures shall be submitted with the building plans and shall be subject to approval by the Community Development Director or his designee.
- 13. All existing and/or new landscaping shall be installed with automatic irrigation

(Adopted by Planning Commission Resolution 00-000)

systems.

- 14. All walls/fences and exposed retaining walls shall be constructed of decorative materials which include but are not limited to splitface block, slumpstone, stuccoed block, brick, wood, crib walls or other similar materials as determined by the Development Review Committee, but specifically excluding precision block.
- 15. The following areas shall be placed in the Landscape and Lighting District:

The developer shall install all improvements and landscape areas. City acceptance on behalf of the Landscape and Lighting District shall be subject to the approval of the Public Works Street Department (237-3864).

- 16. All parking lot landscape planters shall have a minimum outside dimension of six feet and shall be separated from parking and driving areas by a six inch high solid concrete curb.
- 17. The following areas shall be permanently maintained by the property owner, Homeowners' Association, or other means acceptable to the City:

- 18. It is the property owner's responsibility to insure that all construction of private property improvements occur on private property. It is the owner's responsibility to identify the property lines and insure compliance by the owner's agents.

B. THE FOLLOWING CONDITIONS SHALL BE COMPLETED PRIOR TO THE ISSUANCE OF BUILDING PERMITS:

- 1. Two sets of the revised Planning Commission approved plans incorporating all Conditions of Approval, standard and site specific, shall be submitted to the Community Development Department prior to the issuance of building permits.

(Adopted by Planning Commission Resolution 00-000)

- 2. Prior to the issuance of building permits, the
 - Development Review Committee shall approve the following:
 - Planning Division Staff shall approve the following:
 - a. A detailed site plan indicating the location of all structures, parking layout, outdoor storage areas, lighting, walls, fences and trash enclosures;
 - b. A detailed landscape plan;
 - c. Detailed building elevations of all structures indicating materials, colors, and architectural treatments;
 - d. Other: See CUP 07-019 Resolution for specific Planning Division Staff review requirements.

- 3. The applicant shall meet with the City's Crime Prevention Officer prior to the issuance of building permits for recommendations on security measures to be incorporated into the design of the structures to be constructed. The applicant is encouraged to contact the Police Department at (805) 237-6464 prior to plan check submittal.

C. THE FOLLOWING CONDITIONS SHALL BE COMPLETED PRIOR TO OCCUPANCY:

- 1. Occupancy of the facility shall not commence until such time as all Uniform Building Code and Uniform Fire Code regulations have been complied with. Prior to occupancy, plans shall be submitted to the Paso Robles Fire Department and the Building Division to show compliance. The building shall be inspected by the appropriate department prior to occupancy.

- 2. All public or private manufactured slopes located adjacent to public right-of-ways on property in excess of six (6) feet in vertical height and of 2.5:1 or greater slope shall be irrigated and landscaped for erosion control and to soften their appearance as follows: one 15-gallon tree per each 250 square feet of slope area, one 1-gallon or larger size shrub per each 100 square feet of slope area, and appropriate ground cover. Trees and shrubs shall be staggered in clusters to soften and vary the slope plane. Slope planting shall include a permanent irrigation system be installed by the developer prior to occupancy. In lieu of the above planting ratio, the applicant may submit a slope planting plan by a licensed landscape architect or contractor providing adequate landscaping, erosion control and slope retention measures; the slope planting plan is subject to approval by the Development Review Committee. Hydroseeding may be considered on lots of 20,000 square feet or greater.

(Adopted by Planning Commission Resolution 00-000)

PUBLIC WORKS DEPARTMENT - The applicant shall contact the Engineering Division, (805) 237-3860, for compliance with the following conditions:

APPLICANT: Steve Elsayed PREPARED BY: JF

REPRESENTATIVE: _____ CHECKED BY: _____

PROJECT: PD 07-019 TO PLANNING: _____

All conditions marked are applicable to the above referenced project for the phase indicated.

D. PRIOR TO ANY PLAN CHECK:

- 1. The applicant shall enter into an Engineering Plan Check and Inspection Services Agreement with the City.

E. PRIOR TO ISSUANCE OF A GRADING PERMIT:

- 1. Prior to approval of a grading plan, the developer shall apply through the City, to FEMA and receive a Letter of Map Amendment (LOMA) issued from FEMA. The developer's engineer shall provide the required supporting data to justify the application.
- 2. The proposed structures and grading shall not encroach into the 100-year floodway as specified in Municipal Code Chapter 21.14 "Flood Damage Prevention Regulations".
- 3. Any existing Oak trees located on the project site shall be protected and preserved as required in City Ordinance No. 553, Municipal Code No. 10.01 "Oak Tree Preservation", unless specifically approved to be removed. An Oak tree inventory shall be prepared listing the Oak trees, their disposition, and the proposed location of any replacement trees required. In the event an Oak tree is designated for removal, an approved Oak Tree Removal Permit must be obtained from the City, prior to removal.
- 4. A complete grading and drainage plan prepared by a registered civil engineer shall be included with the improvement plans. Drainage calculations shall be submitted, with provisions made for on-site detention/ retention if adequate disposal facilities are not available, as determined by the City Engineer.

(Adopted by Planning Commission Resolution 00-000)

- 5. A Preliminary Soils and/or Geology Report shall be prepared by a registered engineer for the property to determine the presence of expansive soils or other soils problems and shall make recommendations regarding grading of the proposed site.

F. PRIOR TO ANY SITE WORK:

- 1. All off-site public improvement plans shall be prepared by a registered civil engineer and shall be submitted to the City Engineer for review and approval. The improvements shall be designed and placed to the Public Works Department Standards and Specifications.
- 2. The applicant shall submit a composite utility plan signed as approved by a representative of each public utility, together with the improvement plans. The composite utility plan shall also be signed by the Water, Fire, Wastewater, and Street Division heads.
- 3. Any grading anticipated during the rainy season (October 15 to April 15) will require the approval of a Construction Zone Drainage and Erosion Control Plan to prevent damage to adjacent property. Appropriateness of areas shall be subject to City Engineer approval.
- 4. Any construction within an existing street shall require a Traffic Control Plan. The plan shall include any necessary detours, flagging, signing, or road closures requested. Said plan shall be prepared and signed by a registered civil or traffic engineer.
- 5. Landscape and irrigation plans for the public right-of-way shall be incorporated into the improvement plans and shall require a signature of approval by the Department of Public Works, Street Superintendent and the Community Development Department.
- 6. The owner shall offer to dedicate and improve the following street(s) to the standard indicated:

<u>15th Street</u>	<u>Westside</u>	<u>A-12</u>
Street Name	City Standard	Standard Drawing No.

- 7. The owner shall offer to dedicate to the City the following easement(s). The location and alignment of the easement(s) shall be to the description and satisfaction of the City Engineer:
 - a. Public Utilities Easement;

(Adopted by Planning Commission Resolution 00-000)

- b. Water Line Easement;
- c. Sewer Facilities Easement;
- d. Landscape Easement;
- e. Storm Drain Easement.

G. PRIOR TO ISSUANCE OF A BUILDING PERMIT:

- 1. A final soils report shall be submitted to the City prior to the final inspection and shall certify that all grading was inspected and approved, and that all work has been done in accordance with the plans, preliminary report, and Chapter 70 of the Uniform Building Code.
- 2. The applicants civil and soils engineer shall submit a certification that the rough grading work has been completed in substantial conformance to the approved plans and permit.
- 3. When retaining walls are shown on the grading plan, said walls shall be completed before approval of the rough grade, and prior to issuance of any building permits, unless waived by the Building Official and the City Engineer.
- 4. All property corners shall be staked for construction control, and shall be promptly replaced if destroyed.
- 5. Building permits shall not be issued until the water system has been completed and approved, and a based access road installed sufficient to support the City's fire trucks per Fire Department recommendation.
- 6. The developer shall annex to the City's Landscape and Lighting District for payment of the operating and maintenance costs of the following:
 - a. Street lights;
 - b. Parkway and open space landscaping;
 - c. Wall maintenance in conjunction with landscaping;
 - d. Graffiti abatement;
 - e. Maintenance of open space areas.
- 7. Prior to the issuance of a Building Permit for a building within Flood Insurance Rate Map (FIRM) - in zones A1-A30, AE, AO, AH, A, V1-V30, VE and V - the developer shall provide an Elevation Certificate in accordance with the National Flood Insurance Program. This form must be completed by a land surveyor, engineer or architect licensed in the State of California.
- 8. Prior to the issuance of a Building Permit for a building within Flood Insurance Rate

(Adopted by Planning Commission Resolution 00-000)

Map (FIRM) in zones A1-A30, AE, AO, AH, A, V1-V30, VE and V, the developer shall provide a Flood Proofing Certificate in accordance with the National Insurance Program. This form must be completed by a land surveyor, engineer or architect licensed in the State California.

H. PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY:

- 1. The applicant shall pay any current and outstanding fees for Engineering Plan Checking and Construction Inspection services and any outstanding annexation fees.
- 2. No buildings shall be occupied until all public improvements are completed and approved by the City Engineer, and accepted by the City Council.
- 3. All final property corners and street monuments shall be installed before acceptance of the public improvements.
- 4. All top soil removed shall be stockpiled and evenly distributed over the slopes and lots upon completion of rough grading to support hydroseeding and landscaping. All slope areas shall be protected against erosion by hydroseeding or landscaping.
- 5. The applicant shall install all street names, traffic signs and traffic striping as directed by the City Engineer.
- 6. If the adjoining existing City street is inadequate for the traffic generated by the project, or will be severely damaged by the construction, the applicant shall remove the entire roadway and replace it with a minimum full half-width street plus a 12' wide travel lane and 8' wide graded shoulder adequate to provide for two-way traffic. (A finding of "rough proportionality" has been made in the resolution for this condition).
- 7. If the development includes a phased street construction along the project boundary for future completion by the adjacent property owner, the applicant shall provide a minimum half-width street plus a 12' wide travel lane and 4' wide graded shoulder adequate for two-way traffic. (A finding of "rough proportionality" has been made in the resolution for this condition).
- 8. When the project fronts on an existing street, the applicant shall pave-out from the proposed curb to the edge of pavement if the existing pavement section is adequate, and shall feather the new paving out to the centerline for a smooth transition. If the existing pavement is inadequate, the roadway shall be replaced to centerline and the remaining pavement shall be overlaid. (A finding of "rough proportionality" has been made in the resolution for this condition).

(Adopted by Planning Commission Resolution 00-000)

- 9. Any utility trenching in existing streets shall be overlaid to restore a smooth riding surface as required by the City Engineer. Boring and jacking rather than trenching may be required on newly constructed or heavily traveled City streets.
- 10. The applicant shall install all utilities (sewer, water, gas, electricity, cable TV and telephone) underground (as shown on the composite utility plan). Street lights shall be installed at locations as required by the City Engineer. All existing overhead utilities adjacent to or within the project shall be relocated underground except for electrical lines 77 kilovolts or greater. All utilities shall be extended to the boundaries of the project. All underground construction shall be completed and approved by the public utility companies, and the subgrade shall be scarified and compacted, before paving the streets.
- 11. Prior to paving any street the water and sewer systems shall successfully pass a pressure test. The sewer system shall also be tested by a means of a mandrel and video inspection with a copy of the video tape provided to the City. No paving shall occur until the City has reviewed and viewed the sewer video tape and has determined that the sewerline is acceptable. Any repair costs to the pipeline including trench paving restoration shall be at the developer's expense.
- 12. A blackline clear Mylar (0.4 MIL) copy and a blueline print of as-built improvement plans, signed by the engineer of record, shall be provided to the City Engineer prior to the final inspection. A reduced copy (i.e. 1" = 100') of the composite utility plan shall be provided to update the City's Atlas Map.
- 13. All construction refuse shall be separated (i.e. concrete, asphalt concrete, wood gypsum board, etc.) and removed from the project in accordance with the City's Source Reduction and Recycling Element.

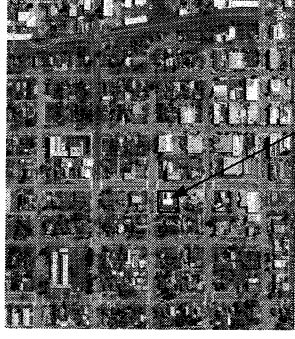
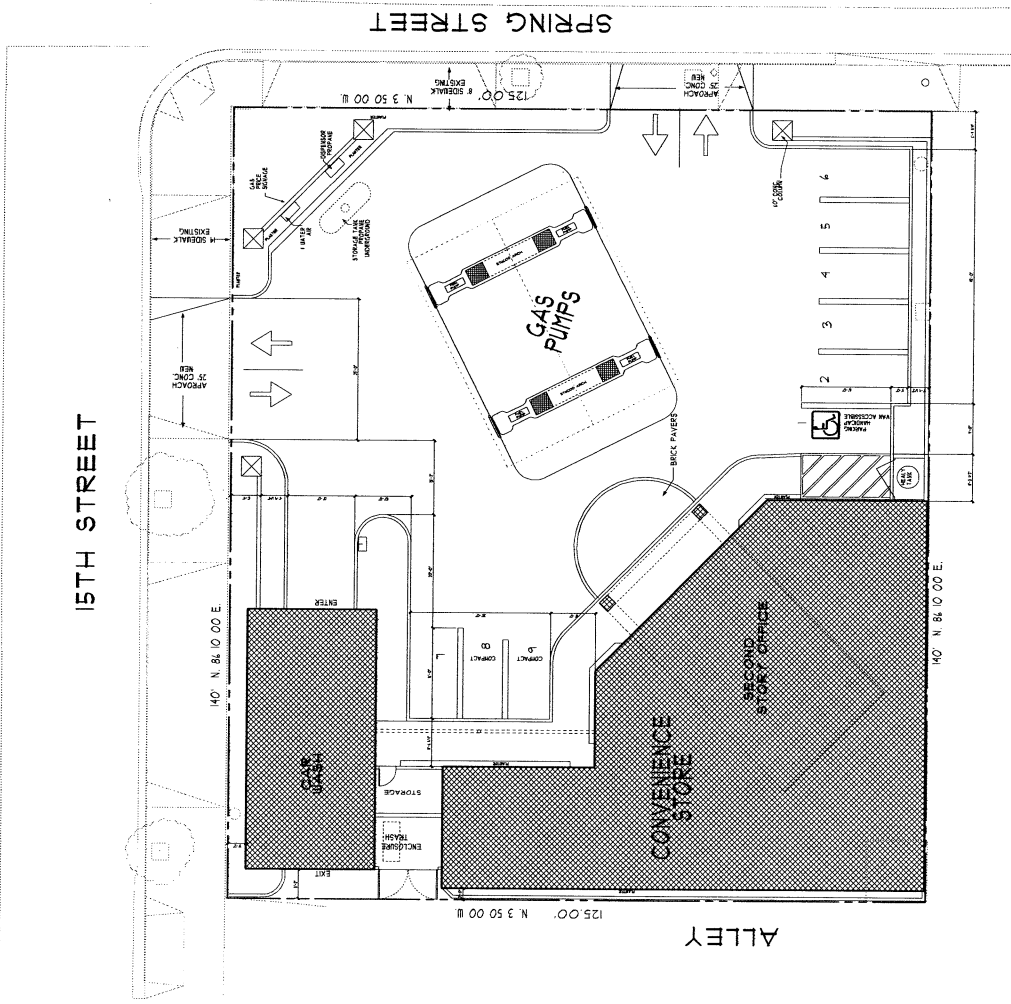
PASO ROBLES FIRE DEPARTMENT - The applicant shall contact the Fire Department, (805) 237-3973, for compliance with the following conditions:

I. GENERAL CONDITIONS

- 1. Fire hydrants shall be installed at intervals as required by the Fire Chief and City Engineer. The maximum spacing for single family residential shall be 500 feet. The maximum spacing for multi-family and commercial/ residential shall be 300 feet. On-site hydrants shall be placed as required by the Fire Chief.
- 2. Building permits shall not be issued until the water system, including hydrants, has been tested and accepted and a based access road installed sufficient to support the City's fire apparatus (HS-20 truck loading). The access road shall be kept clear to a minimum of 24 feet at all times and shall be extended to each lot and shall be maintained to provide all weather driving conditions.
- 3. No buildings shall be occupied until all improvements are completed and accepted by the City for maintenance.
- 4. If the development includes phased street construction, temporary turn-arounds shall be provided for streets that exceed 150 feet in length. The temporary turn around shall meet City requirements as set forth in the Public Works Department Standards and Specifications.
- 5. All open space areas to be dedicated to the City shall be inspected by the Fire Department prior to acceptance. A report shall be submitted recommending action needed for debris, brush and weed removal and tree trimming. The developer shall clean out all debris, dead limbs and trash from areas to be recorded as open space prior to acceptance into a Benefit Maintenance District.
- 6. Any open space included in a private development shall be subject to the approval of a vegetation management plan approved by the Fire Chief.
- 7. Each tract or phase shall provide two sources of water and two points of access unless otherwise determined by the Fire Chief and Public Works Director.
- 8. Provisions shall be made to update the Fire Department Run Book.

(Adopted by Planning Commission Resolution 00-000)

PROJECT DATA
 ADDRESS 141 SPRING STREET
 LOT SIZE = 11,500 SQ.FT.
 BUILDING COVERAGE = 4,555 SQ.FT.
 PAVED AREA = 10,350 SQ.FT.
 LANDSCAPE AREA = 2,245 SQ.FT. 13%
 PARKING PROVIDED =
 1- HANDICAP
 4-4'X18'
 2-8'X12' COMPACT
 BUILDING AREAS
 CONVENIENCE STORE = 4,225 SQ.FT.
 STORAGE = 108 SQ.FT.
 TRASH = 111 SQ.FT.
 CAR WASH = 912 SQ.FT.

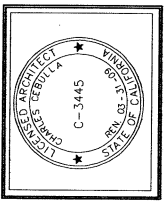


SITE VICINITY MAP



CEBULLA ASSOCIATES
 P.O. BOX 42
 PASADENA, CA 91105-1416
 PH: (805) 413-0216
 E-MAIL: cec@cebullassociates.com
 Architect: CAROL CEBULLA
 Designer: PAT CEBULLA
 Drafter: RON CEBULLA

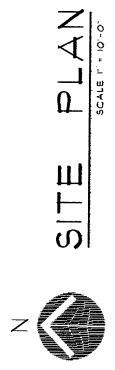
PROJECT:
 NEW SERVICE STATION
 SERVICE BAYS
 SERVICE BAYS STORE
 141 SPRING STREET
 PASADENA, CA 91105
 OWNER:
 STEVE ELBATED
 141 SPRING STREET
 PASADENA, CA 91105
 (805) 238-1415



REVISIONS:

JOB #: 05111
 DATE: 8-2-01
SHEET NO
 1

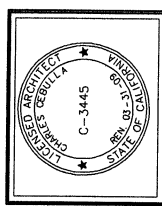
**Exhibit B:
 Site Plan**





CEBULLA ASSOCIATES
 P.O. BOX 47
 PISMO CALIF. 93448
 PH: (805) 713-1298
 FAX: (805) 413-8203
 ARCHITECT: CHARLES CEBULLA
 DRAFTER: HATT CEBULLA

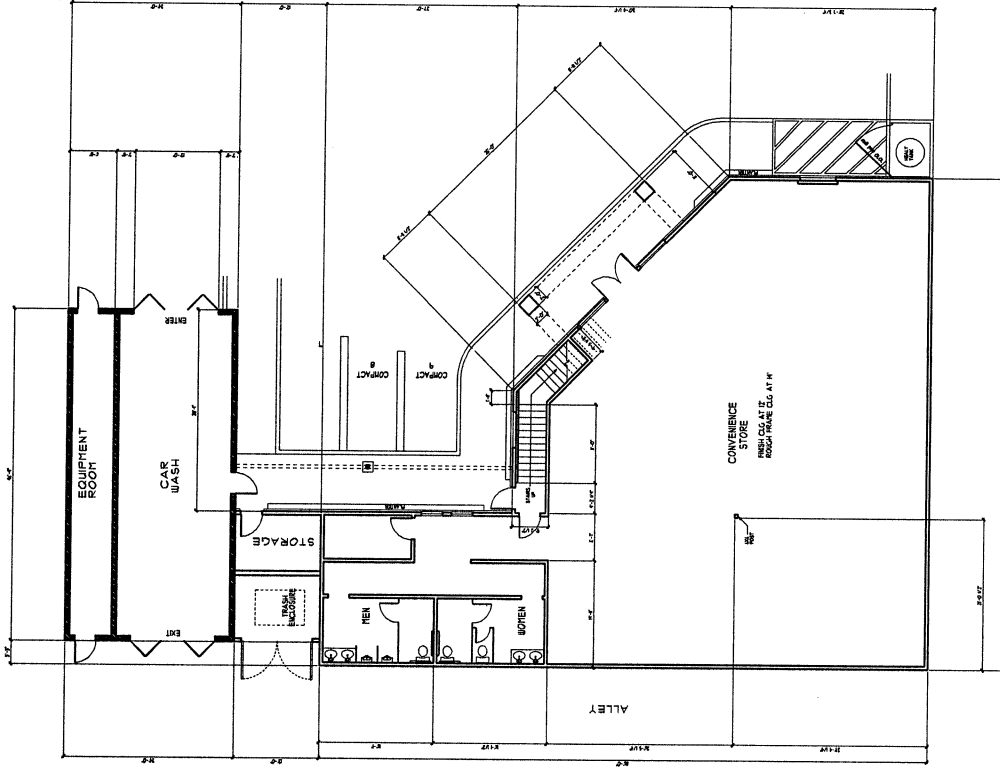
PROJECT:
 NEW SERVICE STATION
 SERVICE STORE
 SERVICE BAYS
 144 SPRING STREET
 PASO ROBLES, CA 93446
 OWNER:
 STEVE BLAYED
 144 SPRING STREET
 PASO ROBLES, CA 93446
 (805) 238-4495



REVISIONS:

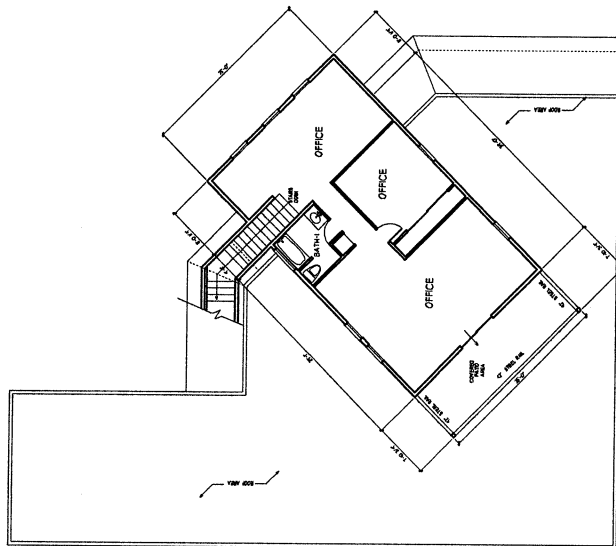
FLOOR PLAN

JOB # 05111
 DATE: 4-2-08
SHEET NO
 2



FIRST FLOOR PLAN

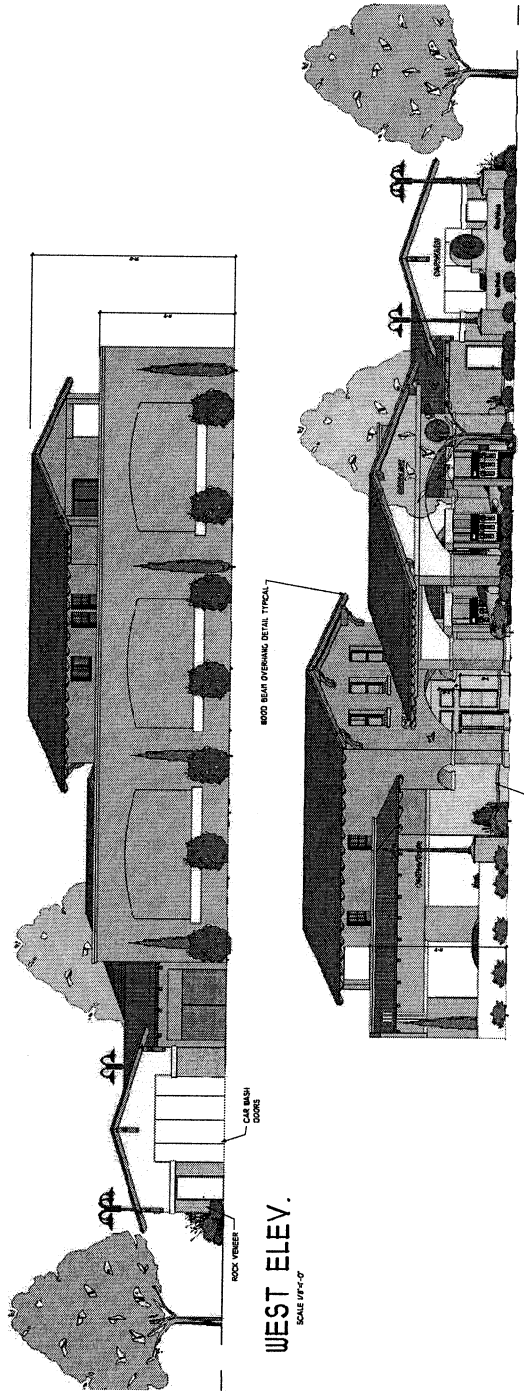
SCALE 1/8" = 1'-0"



SECOND FLOOR PLAN

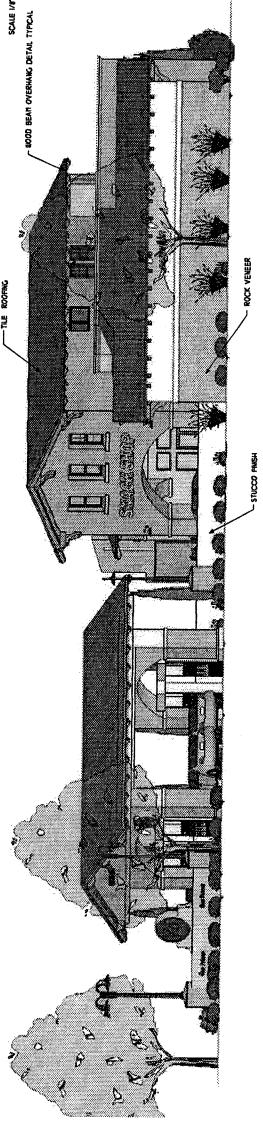
SCALE 1/8" = 1'-0"

**Exhibit C:
 Floor Plans**

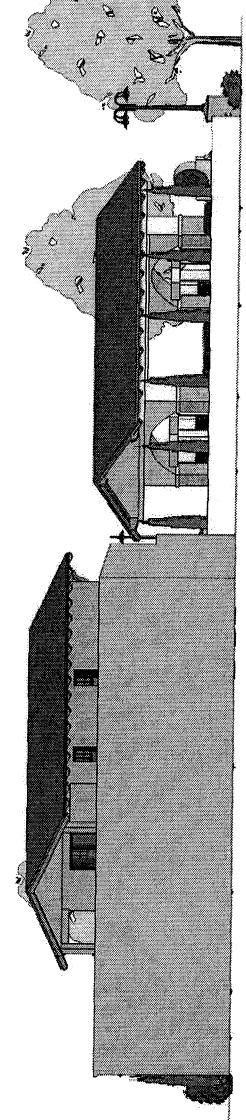


WEST ELEV.
SCALE 1/8"=1'-0"

EAST ELEV.
SCALE 1/8"=1'-0"



NORTH ELEV.
SCALE 1/8"=1'-0"



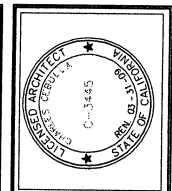
501
SCALE 1/8"

**Exhibit D:
Elevations**



CEBULLA ASSOCIATES
P.O. BOX 42
PISMO BEACH, CA 93446
PHONE: (805) 473-3445
FAX: (805) 473-9203
E-MAIL: cebullagroup@comcast.net
Architect: CHARLES CEBULLA
Designer: JENNIFER CEBULLA
Designer: RON CEBULLA

PROJECT:
NEW SERVICE STATION
SERVICES STORE
144 SPRING STREET
PASO ROBLES, CA 93444
OWNER:
STEVE ELSAYED
144 SPRING STREET
PASO ROBLES, CA 93444
(805) 738-4445



REVISIONS:

JOB #: 08117
DATE: 8-2-07
SHEET NO
3

PROOF OF PUBLICATION

LEGAL NEWSPAPER NOTICES

PLANNING COMMISSION/CITY COUNCIL
PROJECT NOTICING

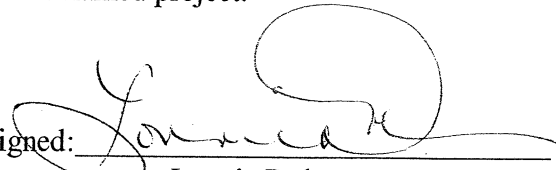
Newspaper: Tribune

Date of Publication: April 2, 2008

Meeting Date: April 22, 2008
Planning Commission

Project: Conditional Use Permit 07-019
(Elsayed - 1441 Spring Street)

I, Lonnie Dolan, employee of the Community Development Department, Planning Division, of the City of El Paso de Robles, do hereby certify that this notice is a true copy of a published legal newspaper notice for the above named project.

Signed: 
Lonnie Dolan

forms/newsaffi.691

Attachment 5:
Affidavits

CITY OF EL PASO DE ROBLES

NOTICE OF INTENT
TO ADOPT A MITIGATED
NEGATIVE DECLARATION

NOTICE IS HEREBY GIVEN that the Planning Commission of the City of El Paso de Robles will hold a Public Hearing to consider adoption of a Mitigated Negative Declaration (statement that there will be no significant environmental effects because of the required mitigation measures) in accordance with the provisions of the California Environmental Quality Act (CEQA), and approval of a Conditional Use Permit for the following project:

Conditional Use Permit 07-019: Conditional Use Permit application filed by Cebulla Associates on behalf of Steve Elsayed, for the installation of a new automated car wash in conjunction with the rebuilding of an existing gas station. The site is located at 1441 Spring Street. (APN: 008-316-016 and 015)

The public review period for the Draft Negative Declaration commences on April 2, 2008, and ends at the Public Hearing, which is scheduled for the Planning Commission on Tuesday, April 22, 2008.

The meeting will begin at the hour of 7:30 pm in the Conference Center (First Floor) at the Paso Robles Library/City Hall, 1000 Spring Street, Paso Robles, California. All interested parties may appear and be heard at this hearing.

The proposed Mitigated Negative Declaration may be reviewed at the Community Development Department, 1000 Spring Street, Paso Robles, California. Copies may be purchased for the cost of reproduction.

Written comments on the proposed conditional use permit and mitigated negative declaration may be mailed to the Community Development Department, 1000 Spring Street, Paso Robles, CA 93446 provided that such comments are received prior to the time of the public hearing. Oral comments may be made at the hearing. Should you have any questions regarding this application, please call Mathew Fawcett at (805) 237-3970.

If you challenge the conditional use permit or mitigated negative declaration in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the Planning Commission at, or prior to, the public hearing.

Mathew Fawcett, Assistant Planner

April 2, 2008

6709700

**AFFIDAVIT
OF MAIL NOTICES**

PLANNING COMMISSION/CITY COUNCIL PROJECT NOTICING

I, Mathew Fawcett, employee of the City of El Paso de Robles, California, do hereby certify that the mail notices have been processed as required for Conditional Use Permit 07-019 (Elsayed – 1441 Spring Street) on this 1st day of April 2008.

City of El Paso de Robles
Community Development Department
Planning Division

Signed: 
Mathew Fawcett

forms/mailaffi.691